

Roll-Out

The Committee had a variety of opinions on the roll-out including the option of covering the entire Village at one time. This option would be aided dependent on grant and loan opportunities that would allow for the Village to finance the entire project and make assessments over ten years.

The Committee was very certain of the importance to provide for a 10-year assessment option. This option is possible, only if the records on the assessments are maintained by the Village and the assessment is certified to the county auditor each year of the 10 years.

Ultimately, if this is not completed as one giant project, there are 6 areas designed to provide for continued application as the community grows. Those areas are labeled A, B, C, D, E and F. From there the committee used a random number generator to assign the order in which enforcement would be rolled out if the plan ultimately involved a system of areas being done over time.

Any roll-out would start with voluntary compliance in 2019 and enforcement being delayed until 2020 to allow for proper notification to be given to home owners in the community.

In 2018, the Village released a sidewalk project spanning private sidewalk construction and community sidewalk installation. This project's bid packet and all other documents provide an example of the potential involvement of the Village on an annual basis to get the projects completed on private and public property. This project also will test the ability of the Village to incorporate the Leipsic Walks Grant Program, and private pay by residents. In addition, the Village is purchasing the concrete directly from K & L Ready Mix hence serving for several levels of billings and payments that will need to be organized.

The response to this packet will show the effectiveness of the existing mindset and thought process and allow for any confusing language to be adjusted before launching the full-scale program.

Random Order of Enforcement





142 E. Main Street
 Leipsic, Ohio 45856
P: (419) 943-2009
F: (419) 943-2010
E: Village@Leipsic.com

Village of Leipsic Sidewalk Bidding Belmore St. and Water Plant
 Bids to Be Accepted by the Village Administrator Through
 September 7, 2018 at Noon

No public bid opening, bids will remain confidential until September 7, 2018, but will be reviewed by the administrator on a rolling basis for planning purposes.

Acknowledgement Form

All qualifying bids must be accompanied by this signed acknowledgement form and must be received by the stated deadline. No bids will be accepted after the deadline. No incomplete or partial bids will be accepted. Bids must be returned on the form included and labeled "Village of Leipsic Sidewalk Bid Sheet 2018 Belmore St. Water Plant Sidewalks."

The Village of Leipsic is managing this sidewalk project for bidding. All work on private property will be handled as a separate agreement between the property owner and the winning contractor. Failure to complete this bid process or completion of this bid process has no impact on the potential for work on the sidewalks contained on private property. The Village has no responsibility for any costs occurred by private property owners or contractors when working on private property.

The Village of Leipsic will be working with K & L Ready Mix for the purchase of all concrete for this project. The Village and the private property owners will pay K & L Ready Mix directly for all concrete and ADA Mats to be purchased for this project.

The Village of Leipsic will handle all grass seeding at the completion of this project or in the following spring. Any contractor submitting a bid, is agreeing to complete the project at the amount bid or less. Responsibilities include all coordination with K & L and concrete ordering, labor, rebar, stone, excavation, site prep, forming, cutting, finishing and any other miscellaneous costs. The contractor is agreeing to conform with all expectations established in the sidewalk standards document included with this bid packet.

The Village of Leipsic reserves the right to award or not award any or all parts of this project. The Village and the private property owners may opt to use different contractors on any portion of this project. The only portions of this project which will be maintained as one unit have an asterisk (*) next to them on the bid sheet.

By completing this bid packet, the contractor is agreeing to provide copies of all relevant, licensing, insurance and bonding that may be needed to complete a project of this size and scope.

 (Signature Authorized Contractor Representative)

 (Name)

 (Company Name)

____/____/____
 (Date)

Mayor: Kevin J. Benton • **Council President:** Susan Schroeder
Council: Rick Recker • Al Pingle • Brian Reynolds • Sue Christman • Dave Heitmeyer
Administrator: Justin Barnhart • **Fiscal Officer/Tax Administrator:** Renee Spangler

Village of Leipsic Sidewalk Bid Sheet 2018 Belmore St. and Water Plant Sidewalks

Directions: Please use this sheet to insert bid estimates for the completion of these projects. The Village of Leipsic will be handling the purchase of the concrete and the ADA Mat from K and L Ready Mix per a special contract. The contractor is responsible for the coordination with K and L and concrete ordering, labor, rebar, stone, excavation, site prep and any other miscellaneous costs. The Village of Leipsic will handle grass seeding after the concrete work is completed. The contractor must complete this work in accordance with the specifications included in the bid packet. **BECAUSE THIS PROJECT HAS ALTERNATES AND PERSONAL PROPERTIES INVOLVED, ONLY THOSE WITH * ARE FOR SURE GOING TO BE INCLUDED IN THE BID AWARD.**

Location	Feet	Yards of concrete the Village plans to need.	Contractor bid per linear foot for all coordination with K and L and concrete ordering, labor, rebar, stone, excavation, site prep and any other miscellaneous costs.	Contractor Bid W/O Concrete or Mats
Project A				
Water Plant Project A--Standard Sidewalk w/ 2 ADA Ramps (Mats purchased by Village)	188.50	9.5		
Water Plant Project A--6 Inch Sidewalk	13.50	1.2		
Project A Total Bid				
PROJECT B				
Project B-1 Church of Nazarene				
Project B-1 Church of Nazarene	141.10	7.2		
Project B-1 Alt 1 Naz Center St.	146.80	7.4		
Project B-1 Alt 2 Church Naz Front Door	22.00	1.0		
Project B-1 Total				
Project B-2 Schey Building				
Project B-2 Schey Belmore Sidewalk	152.00	7.7		
Project B-2 Alt 1 Center Street Sidewalk	110.00	5.6		
Project B-1 Total				
Project B-3 Sidewalk Repair*	9.00	0.6		
Anticipated Project B-3 Contractor Bid				
PROJECT C				
Street Garage Project C-2*	104.00	5.3		
Project C-1 Sidewalk Repair*	4.00	0.4		
Project C-1&2 Total				

	TOTAL FOR * PROPERTIES	\$
	TOTAL FOR THE ENTIRE PROJECT	\$

**Not all projects or alternates will be included in the award based on pricing. The Village of Leipsic and the private property owners reserve the right to award the bids to different contractors based on the price for the bid. Only the bids with * are for sure going to be included in the bid award. The properties with the * will be awarded as one unit regardless of the costs per project. The Village of Leipsic and the personal property owners involved with this project reserve the right to not accept any of the bids submitted and may rebid this project at a future date if that is determined to be beneficial.*

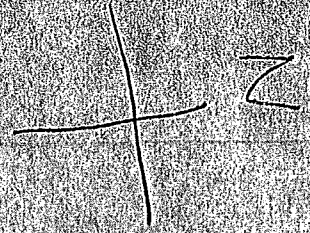
A

1201 St
1100 St

Leipsic Public School

St. Mary's Catholic Rectory

Pills



W Sugar St
W Center St
W Main St
W Broadway St
N Franklin St
N Poplar St
N 7th St

St. Mary's
Blaine St
St. Mary's

Seaton St

E Sugar St
E Center St
E Broadway St

Love-Helmeyer Funeral Home

B

115 St
5th St

Leipsic

Baltimore St

Liberty St

S Engle St

Vine St

Main Cross St

Ohio St

Willow St

Green St
Lacey St
Ragan St

Buckeye Park

Emery St

Prospect St

E. Lincoln Dr
E. Market Dr

5

Q

619
58 Feet Including ADA

IRANDA
FRONT
SIDE

Water Plant Project A

Two ADA Ramps

144 Ft. Incl Drive

100

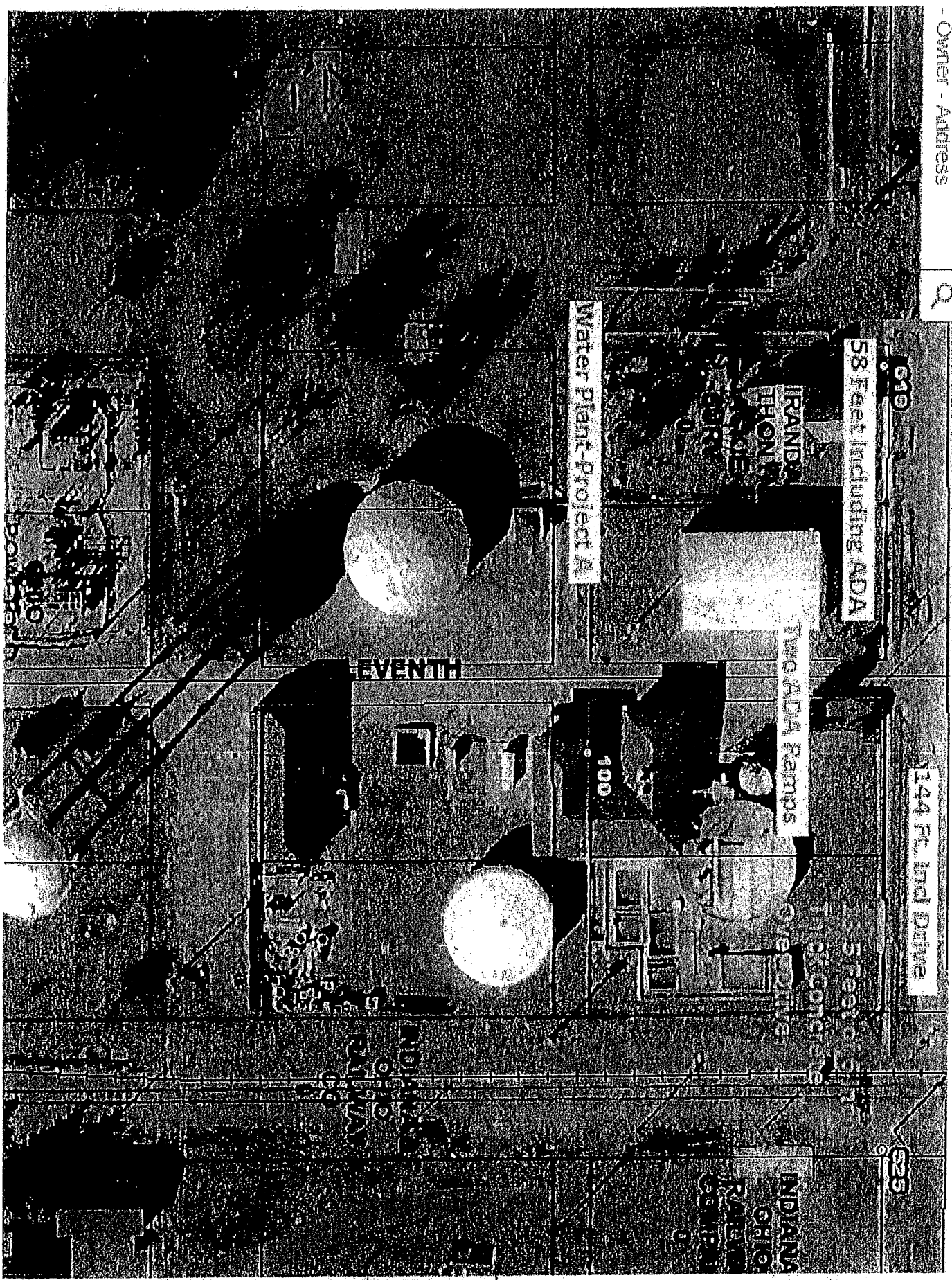
15 Feet
Concrete

525

INDIANA
OHIO
RAILWAY
CORPORATION

SEVENTH

N



IMPORTANT DEADLINES



SEPTEMBER 7, 2018—Have Bid Submitted

October 10, 2018—Begin Work

October 30, 2018—Substantial Completion

November 15, 2018—Punch List Completed

ALTERNATE OPTION FOR LOWER PRICING

Given full schedules for this time of the year, the Village will review alternate pricing in the form of a second bid sheet. If alternate pricing is accepted the following deadlines will be followed.



April 25, 2019 Begin Work



May 15, 2019 Substantial Completion



May 30, 2019 Punch List Completed

Failure to meet these deadlines if awarded the bid will make the contractor liable for damages and may result in the forfeiture of the project.

Sidewalk Standards

Sidewalk standards prepared by Bockrath and associates in 2017 were compared with other documents outlining standards and the standards from Bockrath are complete, accurate and provide for the installation of quality sidewalks that should last.

These standards would need to be adopted by the council to hold any legal wait within the Village.

Village of Leipsic

Sidewalk Construction Standards

Authority

Leipsic Code of Ordinances
Chapter 94: Streets & Sidewalks

Village of Leipsic
142 East Main St.
Leipsic, Ohio 45856
(419) 943-2009

Prepared By:

Bockrath &

Associates

Engineering and Surveying, LLC

115 S. Fair Avenue, Suite A - Ottawa - Ohio 45875

Phone: 419.523.5789

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Village of Leipsic

Sidewalk Construction Standards

- 1) Public Sidewalk Repair Policy
- 2) Specifications for Public Sidewalk Construction
- 3) Sidewalk Standard Drawing
- 4) Concrete Mix Designs
- 5) Permit Application for Sidewalk Construction
- 6) ODOT Standard Drawings (or most current)
- 7) Leipsic Code of Ordinances Chapter 94

Village of Leipsic

Public Sidewalk Repair Policy

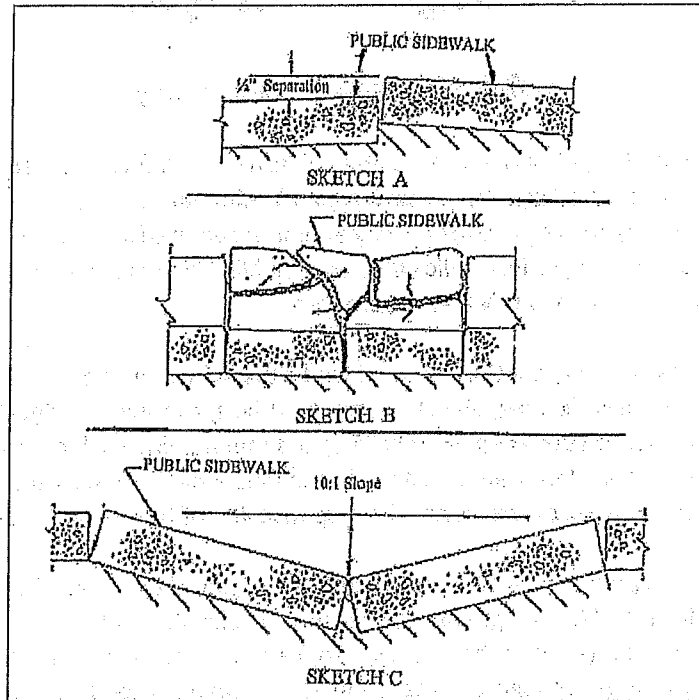
Purpose - The purpose of this policy is to protect the public health, safety and welfare of the citizens and inhabitants of the Village of Leipsic by preventing and eliminating tripping hazards on the public sidewalks to pedestrians, by repairing and/or replacement of required sections of sidewalks, to achieve, as close as possible, a uniform grade. Public sidewalk - "public sidewalk" means any main or approach sidewalk, between the curb line and private property line.

Responsibility - Every owner of any lot or parcel of land situated within the Village of Leipsic shall keep, repair and maintain the sidewalk along all public streets, drives, avenues, boulevards or lanes adjoining such lot or parcel. The cost of such keeping, repairing and maintaining shall be the responsibility of the individual owner or Village as determined. Sidewalk sections which extend from existing sidewalks to the curb to facilitate the crossing of streets are the responsibility of the Village of Leipsic.

Determination factor - A public sidewalk is required to be repaired or replaced when a section(s) has a separation of one-half inch (1/2") or greater in the surface elevation which creates a tripping hazard (See sketch A). Also, a section(s) which has cracks, surface deterioration or separations which create an uneven surface and are large enough to create either a tripping hazard or a possible ankle turning situation must be replaced (See sketch B). Sidewalk sections, which meet with severe slopes or rises, greater than 10:1 shall be repaired as required (See sketch C). Village inspection of sidewalks to identify sidewalks which require repair and/or replacement will be done by walking the designated area and using a one-half inch (1/2") template to measure the amount of separation.

Repair/Replacement - Public sidewalk repair and/or replacement can be accomplished by the following methods:

- 1) Remove the existing damaged section(s) of sidewalk and replace with new section(s) of sidewalk per Village Sidewalk Specifications.
- 2) Raise or lower specific section(s) of sidewalk using compacted berm stone as a subbase for the walk. Sidewalk leveling can also be achieved by concrete leveling contractors who drill holes in sidewalk and pump a concrete or grout mixture under the walk, which raises it and levels the walk.
- 3) Ramping or grinding of sidewalk sections to achieve a uniform grade is prohibited.
- 4) If the section(s) break or separation is not located at a sidewalk joint, then the sidewalk section(s) must be replaced with new sidewalk.
- 5) If a property owner wishes to repair/replace the sidewalk themselves, or hires a contractor to repair/replace the adjacent sidewalk, they must first obtain a sidewalk permit from the Village.
- 6) Sidewalk sections which must be replaced with new concrete must meet the sidewalk specifications of the Village of Leipsic.



Leipsic Walks Sidewalk Grant Program (if Available)

Eligibility: All property owners intending to complete a sidewalk project in the street right-of-way are eligible to apply for the grant. Proper grant application immediately qualifies the applicant for the special bulk concrete pricing for concrete poured in the public right-of-way for sidewalk projects. *This pricing may not be applicable for short loads unless there is an ability to pool several property owners into a group for pouring at one time.*

Grant Recipient Selection: Grant recipients shall be selected based on eligibility and according to those scoring highest on a rubric determined by the Village Administrator, that has been approved by Village Council. Grants shall not be awarded without approval from the Village Council.

Grant Amounts: All grants shall amount to \$5.00 five-dollars per linear feet of sidewalk poured in the public right-of-way for sidewalk installation. This distance shall be verified by the Village Administrator.

Approved Sidewalk Installers: No grant applicant is eligible for grant funds or extension of bulk concrete pricing if they are not using an approved sidewalk installer or installing the sidewalk themselves in accordance with the Village sidewalk specifications. Contractors not already approved, will be approved on a rolling basis upon completion of an application and the findings of the Village Administrator that the contractor is qualified to complete the work in accordance with the specifications of the Village.

Village of Leipsic

Specifications for Sidewalk Construction and Repair

NOTE: A permit is required to construct or reconstruct sidewalks in the Village of Leipsic.

1. Sidewalk Standards

Sidewalk width shall match existing and adjacent sidewalk and be four (4) inches thick, except at drive approaches. Across residential drive approaches, sidewalks shall be six (6) inches thick. Sidewalks shall slope toward the street one quarter inch ($\frac{1}{4}$) per foot. Two (2) inches ODOT item 411 stabilized crushed aggregate, shall be placed under the walk and compacted.

Residential Drive Approaches

All concrete drive approaches shall be six (6) inches thick with four (4) inches of item 304 or item 411 aggregate base. Asphalt drive approaches shall be two (2) inch minimum asphalt with Six (6) inch aggregate base compacted.

2. Excavation, Subgrade & Base

Excavate to required depth and to a width that allows installation and bracing of Forms. All soft spongy material shall be removed and replaced with suitable material and compacted until it is firm. Place and compact two (2) inches of ODOT item 411 stabilized crushed aggregate.

3. Forms

Forms shall be of wood or metal and extend for the full depth of the concrete and shall be of sufficient strength to resist the pressure of the concrete without springing.

4. Reinforcing Steel

Per ACI Code. Deformed Steel Bars size #3 ($3/8"$) and #4 ($1/2"$) or Wire Mesh- 6x6-W2.0x2.0 (8 Gauge or Larger). Clearance between reinforcing steel and surface of the concrete shall not be less than 2-1/2 inches. Note: Fiber Mesh may be added to Concrete Mix to resist shrinkage cracking but does not replace the Reinforcing Steel requirements.

5. Concrete

Sidewalks shall be constructed of Portland Cement Concrete conforming to ODOT Item 499 Specification Class C (600# Cement). ODOT Item 499 Specification Class C Option 3 (470 # Cement) may be used from May 1st to November 1st. Concrete shall test 4,000 psi compressive strength at 28 days. Concrete shall have a maximum of five (5) inch slump. The amount of air entrained in the concrete shall be six (6) percent. In general, materials shall meet item 608.02 of the State of Ohio Department of Transportation Construction and Materials Specifications, except as otherwise provided herein. Color additives are not permitted.

Do not pour concrete on frozen ground or when temperatures will drop below 32° F.

6. Grade

Any sidewalk constructed or reconstructed shall be constructed or reconstructed on such plane inclination and level so that the wearing surface there of shall be uniform and even throughout its length and width and shall be uniform and even with any adjoining and connecting walk. Grade between curb edge and sidewalk edge, shall slope one-half (½) inch per foot toward top of curb. Maximum longitudinal grade of sidewalk is 5.0%

7. Placing & Finishing

Immediately before placing concrete, thoroughly moisten the subgrade. The concrete shall be deposited in a single layer. It shall be struck off with a template and smoothed with a float to obtain a sandy texture. No plastering will be permitted. All outside edges and joints shall be edged with a quarter (¼) inch radius edging tool. Finished Concrete must have a broom finish. No Concrete Stamping.

8. Joints

Joints shall be placed at intervals of five (5) feet and shall be perpendicular to the outside edges of the walk. Joint depth shall be one-quarter (¼) the slab thickness. Pre-molded expansion joints of one-half (½) inch thick shall be placed not more than Fifty (50) feet apart and whenever sidewalk intersect other sidewalks, curbs, drives, etc.

9. Curing

Spray exposed surfaces with a uniform application of curing membrane immediately after finishing surface and free water has dissipated.

10. Sealing

Spray or roll exposed surfaces with a uniform application of Bearcoat Premier or approved equal sealing material to manufactures specifications.

11. Driveways (Residential)

Where there is an existing Portland cement concrete or asphaltic concrete drive in reasonable good condition, the sidewalk may terminate at the edge of such a drive. Where stone or dirt driveways exist or the drive in the area of the sidewalk is not in good condition, then the sidewalk shall be constructed across the driveway and shall be six (6) inches thick. The Village will make the final determination regarding the condition of a drive.

12. Protection of Work

Sidewalks and Driveways under construction shall be barricaded or fenced to protect pedestrians and curing concrete. Damages to uncured concrete are the responsibility of the contractor.

13. Americans with Disabilities Act (ADA) Compliance

All new construction must comply with current ADA Standards for Accessible Design. ADA ramps must be poured 7" thick from the curb extending to the rear edge of the detectable warning pad. Detailed drawings for new curb ramps and combined curb and sidewalk can be found on the world wide web at:

http://www.dot.state.oh.us/Divisions/Engineering/Roadway/DesignStandards/roadway/Standard%20Construct%20Drawings/BP-7.1_2014-07-18.pdf

14. Tree Roots

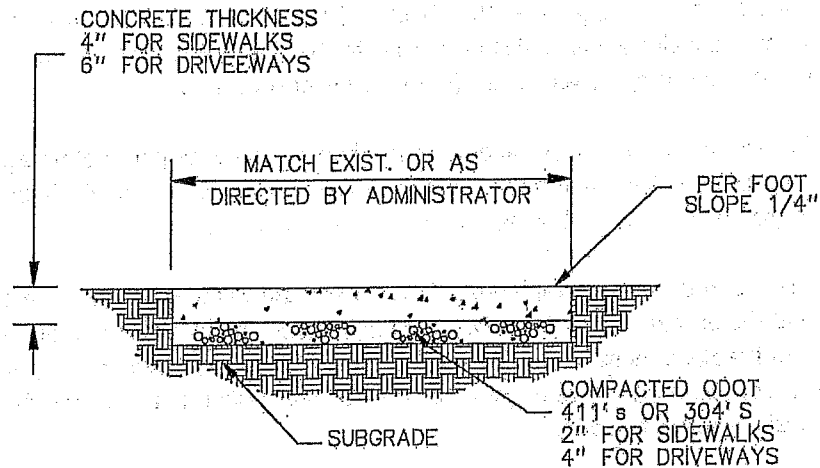
Construction Near Public Trees:

- (a) For all trees measuring twelve (12) inches or less DBH (Diameter at Breast Height - Breast height is defined as 4.5 feet (1.37m) above the forest floor on the uphill side of the tree), soil excavation work or root cutting shall not occur closer than three (3) feet from the outer bark of the tree.
- (b) For trees measuring greater than twelve (12) inches DBH, soil excavation work or root cutting shall not occur closer than a distance equal to the circumference of the tree measured at a height four feet above ground level or six (6) feet, whichever is less.
- (c) Soil excavation work is permitted closer than the distance parameters established in (a) and (b) above provided all excavation of soil is accomplished by hand, shovel, air spade or auger, and no roots greater than two (2) inches in diameter are severed.
- (d) If lateral roots greater than two (2) inches in diameter are encountered, they shall be exposed beyond the excavation limits and an attempt shall be made to bend and relocate the roots without breaking them. If such lateral roots are encountered immediately adjacent to the location of new construction and relocation does not appear practical, the Village Administrator or his or her designee shall be contacted.
- (e) If, after inspection by the Village Administrator or his or her designee, it is determined that the cutting of roots cannot be avoided, the following standards shall be observed: Roots must be cut sharply and cleanly. The wound shall not be painted or treated. All excavated roots shall be backfilled by hand as soon as possible to prevent drying. If immediate backfilling is not possible, roots shall be covered with peat moss or wet burlap and watered. The tree shall be thoroughly watered by providing the equivalent of one (1) inch of water per week for up to one year at the direction of the Village Administrator or his/her designee.
- (f) No construction equipment shall be parked and no particulate construction material such as sand, soil or gravel shall be stored on the surface of any unpaved areas within the drip line of any public tree without the express permission of the Village Administrator or his/her designee.
- (g) If the removal of a public tree cannot be avoided, or if a public tree does not survive due to construction activities, the owner of the facility to be constructed shall obtain a permit and shall remove the tree at the owner's sole expense. Said owner shall also plant, at the owner's sole expense, a replacement tree of a size, type and at a location approved by the Village Administrator.

NOTE: A permit is required to construct or reconstruct sidewalks in the Village of Leipsic.

Village of Leipsic

Standard Sidewalk Construction Details



EXTERIOR CONCRETE:
THICKNESS - AS INDICATED ON PLANS
TYPE - ODOT CLASS "C"
SPECS TO FOLLOW ODOT CMS
JOINT SPACING - CURB (10' MAX.)
SIDEWALKS (5' MAX.)
FINISH - BROOM W/ TOOLED JOINTS &
EDGES
SEALING - BEARCOAT PREMIER OR
APPROVED EQUAL
REINFORCING - PER ACI CODE
(UNLESS SHOWN OTHERWISE)

CONCRETE WALKS, DRIVES AND CURB RAMPS SHALL BE REINFORCED AND DOWELED INTO THE PROPOSED CURB.

TO CLARIFY, THIS APPLIES TO SIDEWALKS, DRIVE APRONS AND CURB RAMPS. THE REINFORCING SHALL BE A #4 REBAR MAT IN THE CENTER OF THE CAST-IN-PLACE SLAB AT MAXIMUM 4 FOOT ON CENTER AND ALSO THE DOWELS SHALL BE AT MAXIMUM 4 FOOT SPACING. THE DOWELS ALSO SHALL BE #4 REBAR AND PLACED INTO 3/4" DRILLED HOLE INTO THE CURB, SIDEWALK, DRIVE, ETC.

Bockrath &

Associates
Engineering and Surveying, LLC
115 S. Fair Avenue, Suite A - Ottawa - Ohio
Phone: (419) 523 - 5789

DATE: 09/21/17

TO: Bockrath Engineering & Associates

FROM: K&L Ready Mix



PROJECT: Village of Leipsic Sidewalks

DESIGN STRENGTH PSI @ 28 day

4000

4000

MIX DESIGN NUMBER:

ODOTCAE

ODOTC3

PLACEMENT TYPE:

Flatwork

Flatwork

MATERIAL	SP. GR.	SSD WT.	SSD WT.
CEMENT	3.15	600	470
FLY ASH	2.66	0	0
GRAN CEM	3.01	0	80
FINE AGGREGATE	2.65	1280	1296
COARSE AGGREGATE	2.67	1741	1782
WATER	1.00	270	250
WATER	gallons	32	30
TOTAL		3891	3878
AIR ENTRAINMENT	%	6	6
SLUMP	MAX	5	5
WATER/CEMENT RATIO	MAX	0.45	0.46
FINE AGGREGATE RATIO	%	0.430	0.423
FINE AGG. MOISTURE	%	2	2
COARSE AGG. MOISTURE	%	0	0
UNIT WEIGHT	lb/cf	144.1	143.6
DESIRED YIELD	cf/cy	27.00	27.00

ADMIXTURE DOSAGE

Con Air ASTM C260	oz/cwt	1.5	1.5
OptiFlo 500 ASTM C494 Type A	oz/cwt	2.0	2.0

MATERIAL	SOURCE	Type
CEMENT	Lafarge Paulding	1
GRAN CEM GGBFS	Lafarge South Chicago Plant	100
FINE AGGREGATE	Weber Sand & Stone Edgerton	N.S.
COARSE AGGREGATE	Putnam Aggregates, Ottawa OH	57'S

Village of Leipsic

142 East Main St.
Leipsic, Ohio 45856

Sidewalk Construction and Repair Permit Application

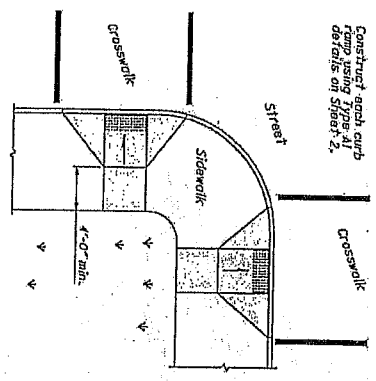
NOTE: A permit is required to construct or reconstruct sidewalks in the Village of Leipsic and no concrete shall be poured until and inspection has been made of subgrade and formwork.

Property Owner _____		
Address of Proposed Sidewalk / Driveway _____		
Mailing Address of Owner (if different) _____		
Phone Number _____		
Legal Description of Property _____		
Concrete Contractor _____	Phone _____	
Sidewalk Construction (circle one)	Replace Existing	New
Sidewalk Location on Lot (circle one)	Front	Side Both
Sidewalk Use (circle one)	Residential	Commercial
Sidewalk Dimensions - Length (ft.) _____	Width (ft.) _____	
Description of Work _____		
Description of Concerns / Obstructions _____		

Village of Leipsic Use Only	
Application No. _____	Date Received _____
Permit Approved By _____	Date _____
Work Completed By _____	Date _____
Inspected By _____	Date _____
Grant Amount Released \$ _____	Date _____

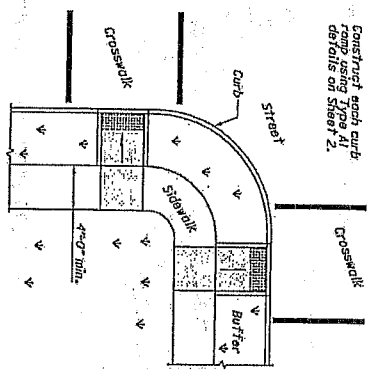


Construct each curb ramp using Type A1 details on Sheet 2.



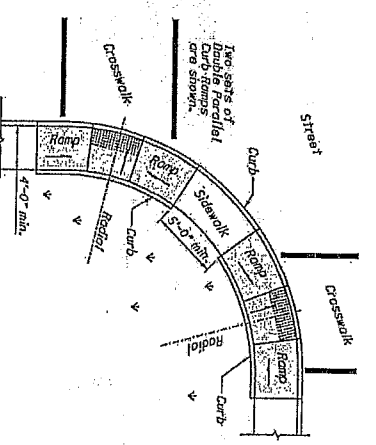
Use curb ramps with flared sides at locations with wide sidewalks.

Construct each curb ramp using Type A2 details on Sheet 2.



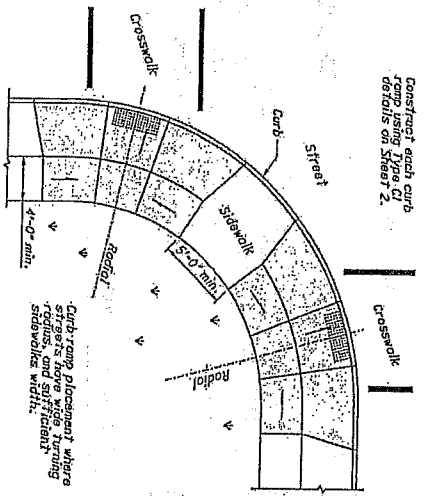
Use curb ramps with no flared sides where buffer is wide enough to accommodate ramp slope.

Use curb ramps with no flared sides where buffer is wide enough to accommodate ramp slope.



Place on streets having wide turning radius, and where sidewalks are narrow.

Construct each curb ramp using Type C1 details on Sheet 2.

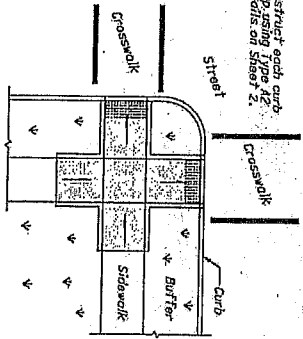


Curb-ramp placement where streets have wide turning radius, and sufficient sidewalk width.

PERPENDICULAR CURB RAMP

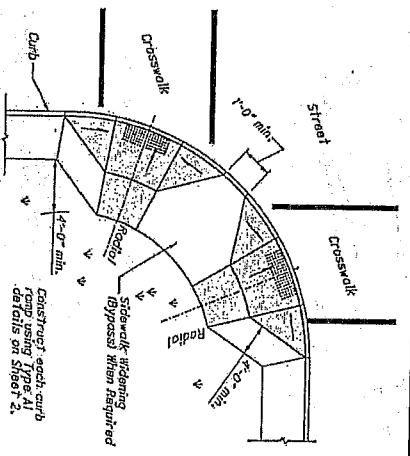
PREFERRED CONSTRUCTION PLACEMENT

COMBINATION CURB RAMP



NOTES

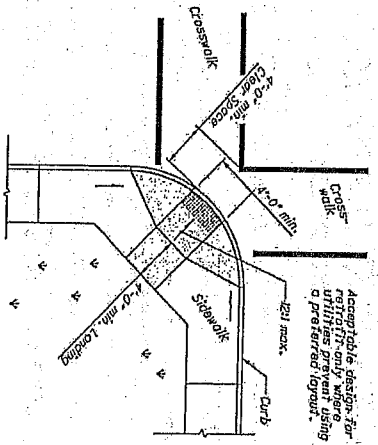
GENERAL: This drawing shows curb ramp types and placement examples for curb ramp construction, including the installation of detectable warnings. Curb ramps are shown on Sheet 2 and include Perpendicular, Parallel, and Combination types. Curb ramps added to an existing intersection or walk should be individually detailed on the project plans to assure that the detectable warning is installed in accordance with the project plans to assure that the detectable warning is installed in accordance with the project plans. **PAYMENTS:** Measure and pay for the ramp area within the shaded limits of this drawing. From each curb ramp, square feet, this includes the cost of transportation, installation, grading, forming, and finishing required within the shaded area. Work beyond the shaded ramp/landing area is paid for as curb (60%) and walk (40%). Removal of all-grade crossing locations where only detectable warnings are required in order to meet ADA requirements, measure and pay for the strip of detectable warnings as forming, and finishing required within the shaded area. The strip of detectable warnings also require removal of existing pavement, if any, to the nearest joint, or if no joint exists, a minimum of 4 feet.



Acceptable design at a corner with wide turning radius. Detectable warning strip is narrower than sidewalk, but so as not to encroach into adjacent traveled lanes.

PERPENDICULAR RAMP

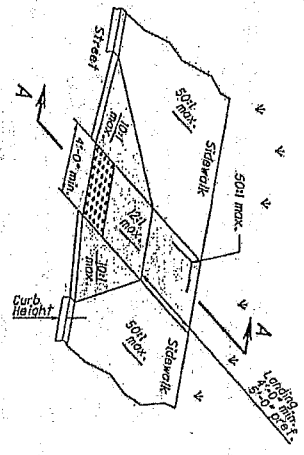
ACCEPTABLE CONSTRUCTION PLACEMENT



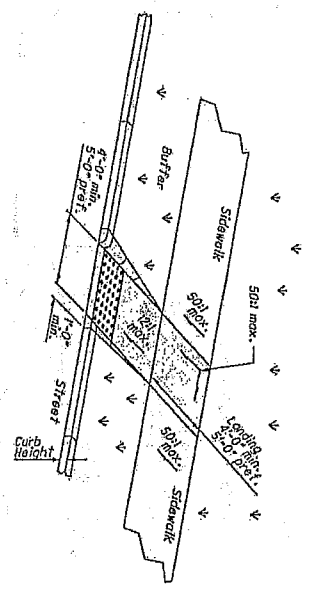
Use this design only for existing walks, and when site constraints prohibit construction of a parallel or combination curb ramp Type A. Avoid using where curb radii are less than 20'-0\"/>

DIAGONAL RAMP (TYPE D)

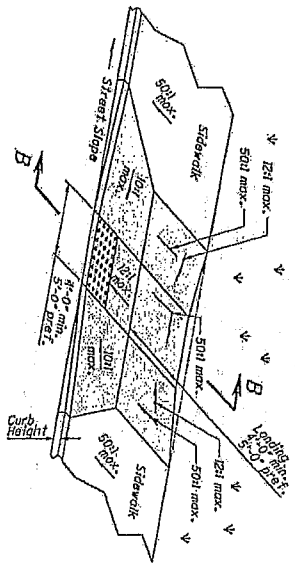
THIS DRAWING REPLACES BP-7.1 DATED 10-15-10.



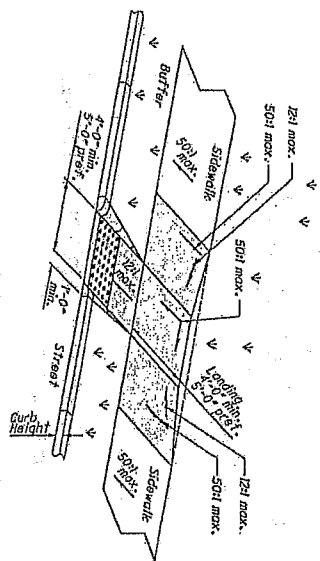
Type A1 (Perpendicular with flared sides)
PERPENDICULAR CURB RAMP DETAILS



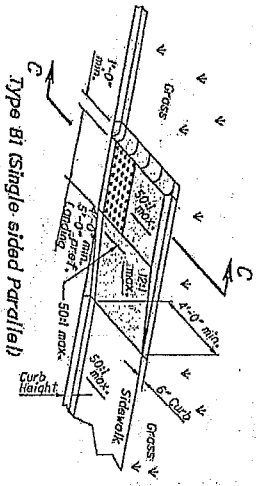
Type A2 (Perpendicular with returned curb)
PERPENDICULAR CURB RAMP DETAILS



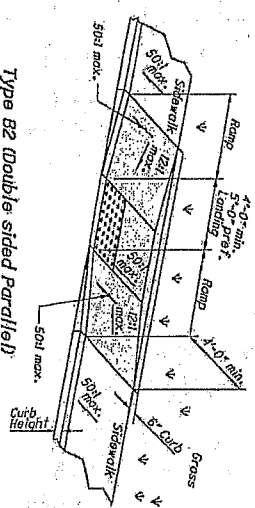
Type C1 (Combined with flared sides)
COMBINED CURB RAMP DETAILS



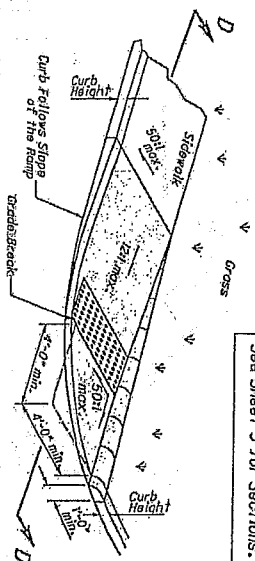
Type C2 (Combined with returned curb)
COMBINED CURB RAMP DETAILS



Type B1 (Single-sided Parallel)
PARALLEL CURB RAMP DETAILS



Type B2 (Double-sided Parallel)
PARALLEL CURB RAMP DETAILS



Type B3 (Single sided Parallel)
PARALLEL CURB RAMP DETAILS

See Sheet 3 for Sections.

NOTES CONTINUED

The running slope of the ramp is preferred to be 1:24 or flatter. In existing conditions, the maximum ramp slope is not possible due to site constraints, the maximum allowable slope for wheelchairs, right-of-way limits, etc. may be reduced as follows:

- A) 1:40 for a max. rise of 7'-6"
- B) 1:48 for a max. rise of 7'-0"
- C) 1:50 for a max. rise of 7'-0"
- D) 1:60 for a max. rise of 6'-0"

To prevent closing the grade inadvertently, the transition from existing sidewalk to the shaded curb ramp area is not required to exceed 1/8 inch in length.

While ramps may be sloped to the crosswalk, the entire lower landing area must fall within the crosswalk that the ramp serves and cannot be located in the traveled lane of opposing traffic.

The counter slope of the gutter or street of the foot of a curb ramp, landing, or blended transitions shall be 20:1 or flatter.

The bottom edge of the ramp shall change planes perpendicular to the landing. The edge of the curb shall be flush with the edge of the adjacent pavement and gutter and surface slopes that meet grade breaks shall also be flush.

Ramp landings shall be 4' min. x 4' min. with a 50:1 or flatter cross slope and running slopes.

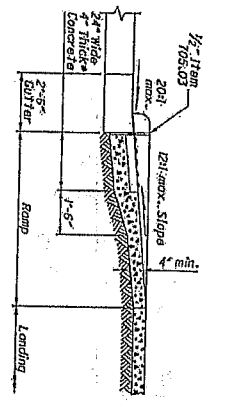
DETECTABLE WARNINGS: Install detectable warnings on each curb ramp with approved materials, as shown on Sheet 3. Install these proprietary products as per manufacturer's written instructions.

DRAINAGE: Contractor is to ensure the base of each completed curb ramp allows for proper drainage, without exceeding allowable cross slope or ramp slopes. Vertical change in level exceeding 1/2" between the pavement and gutter, and 2" gutter and ramp, are not allowed.

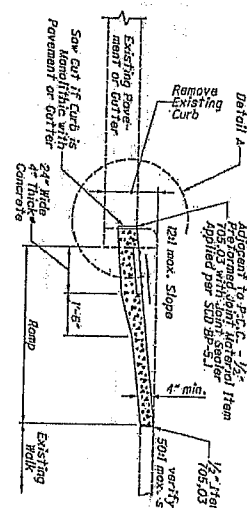
SPALLS/TEXTURE: Textured concrete surfaces by coarse brooming. Transverse joints provide expansion joints in the curb ramp at the expansion of work shall be consistent with Item 606.03 requirements for a new concrete walk or curb. From this expansion joint, filler ground the edge of ramps built in concrete. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide.

JOINTS: Provide expansion joints in the curb ramp at the expansion of work shall be consistent with Item 606.03 requirements for a new concrete walk or curb. From this expansion joint, filler ground the edge of ramps built in concrete. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide. The filler shall be 1/2" thick and 1/2" wide.

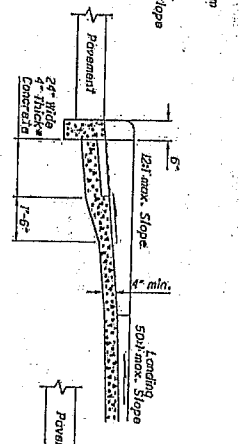
THIS DRAWING REPLACES BP-7.1 DATED 10-15-10.



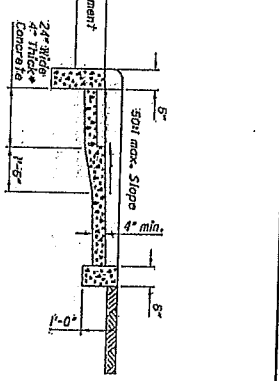
**SECTION A-A
NORMAL DETAIL**
See Sheet 2.



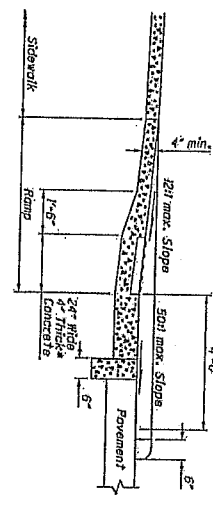
**SECTION A-A
EXISTING WALK DETAIL**
See Sheet 2.



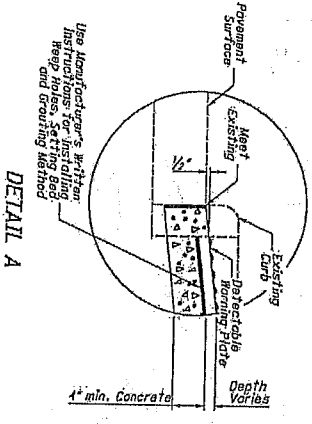
SECTION B-B
See Sheet 2.



SECTION C-C
See Sheet 2.



SECTION D-D
See Sheet 2.
*Where possible, pour ramp area integral with the curb, which will use 6" thick walk.



DETAIL A

DETECTABLE WARNINGS NOTES

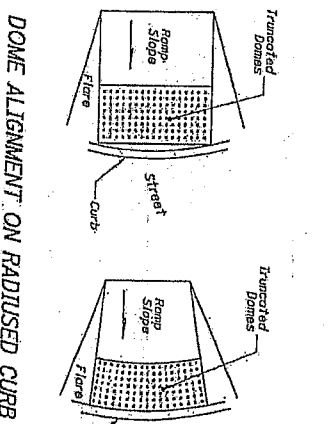
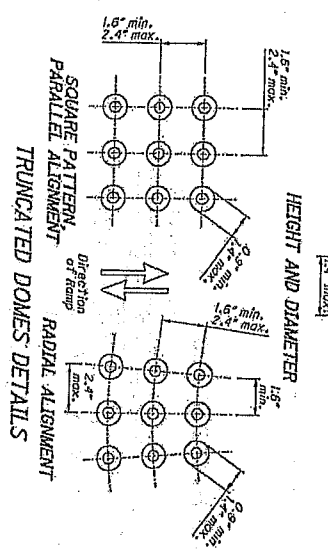
GENERAL: Detectable warnings are a distinctive surface pattern of truncated domes which are detectable by the feet of people with vision impairments of their approach to streets and hazardous objects.

PLACEMENT: Detectable warnings are to be installed of any location where pedestrian-vehicle crossings occur, such as at the intersection of streets, at the corner placement locations, or shown on sheet 1.

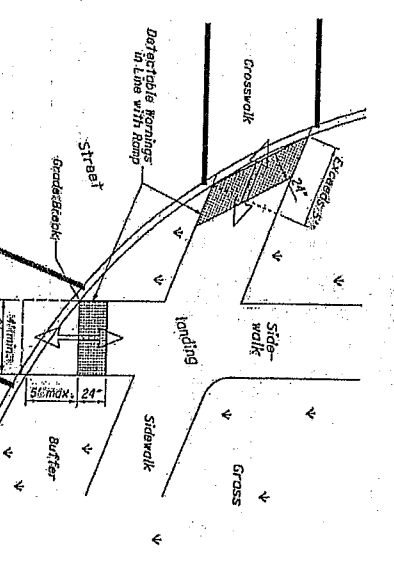
The depth of curbs and underneath detectable warning products shall be a minimum of 4". See DETAIL A.

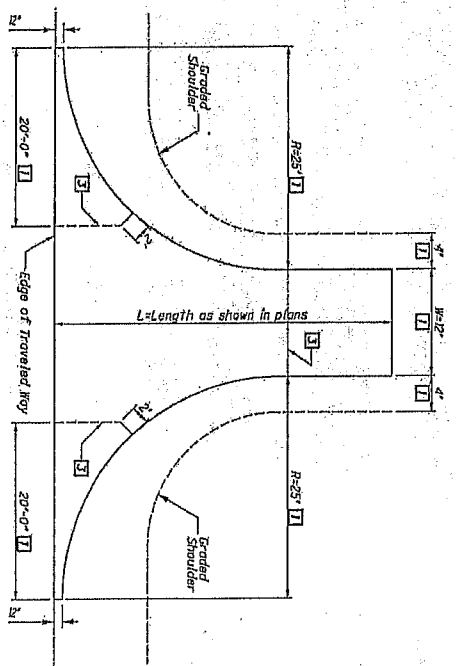
ALIGNMENT: Truncated domes should be aligned with the primary direction of travel. The alignment of the detectable warning alignment shall be normally parallel to the curb. The alignment of the curb shall be normally parallel to the curb. The alignment of the curb shall be normally parallel to the curb.

PRODUCTS & COLORS: Color of the detectable warning should contrast with surrounding pavement. The color of the curb should be approved by the engineering services. The color of the curb should be approved by the engineering services. The color of the curb should be approved by the engineering services.

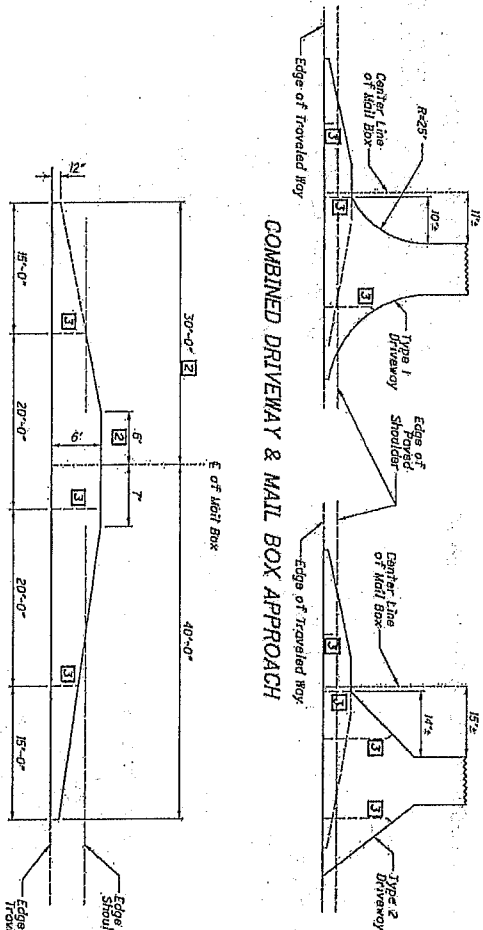


DETECTABLE WARNING ALIGNMENT



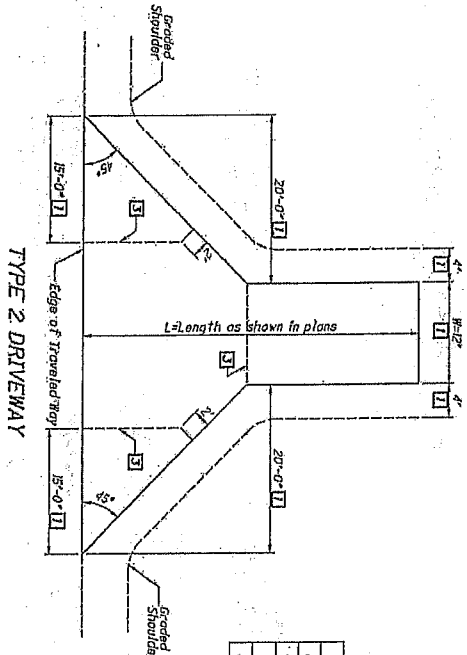


TYPE 1 DRIVEWAY

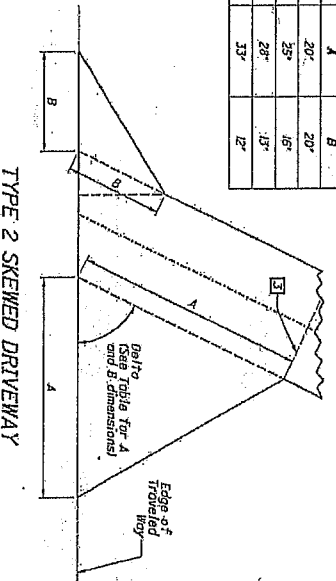


TYPICAL MAIL BOX APPROACH

COMBINED DRIVEWAY & MAIL BOX APPROACH



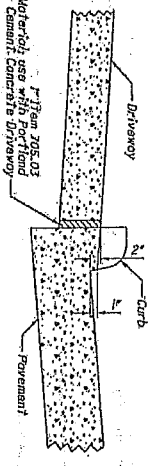
TYPE 2 DRIVEWAY



TYPE 2 SKEWED DRIVEWAY

Delta	A	B
60° To 90°	20'	20'
75° To 85°	25'	16'
85° To 95°	28'	15'
55° To 65°	13'	12'

DROP CURB DETAILS AT DRIVEWAYS



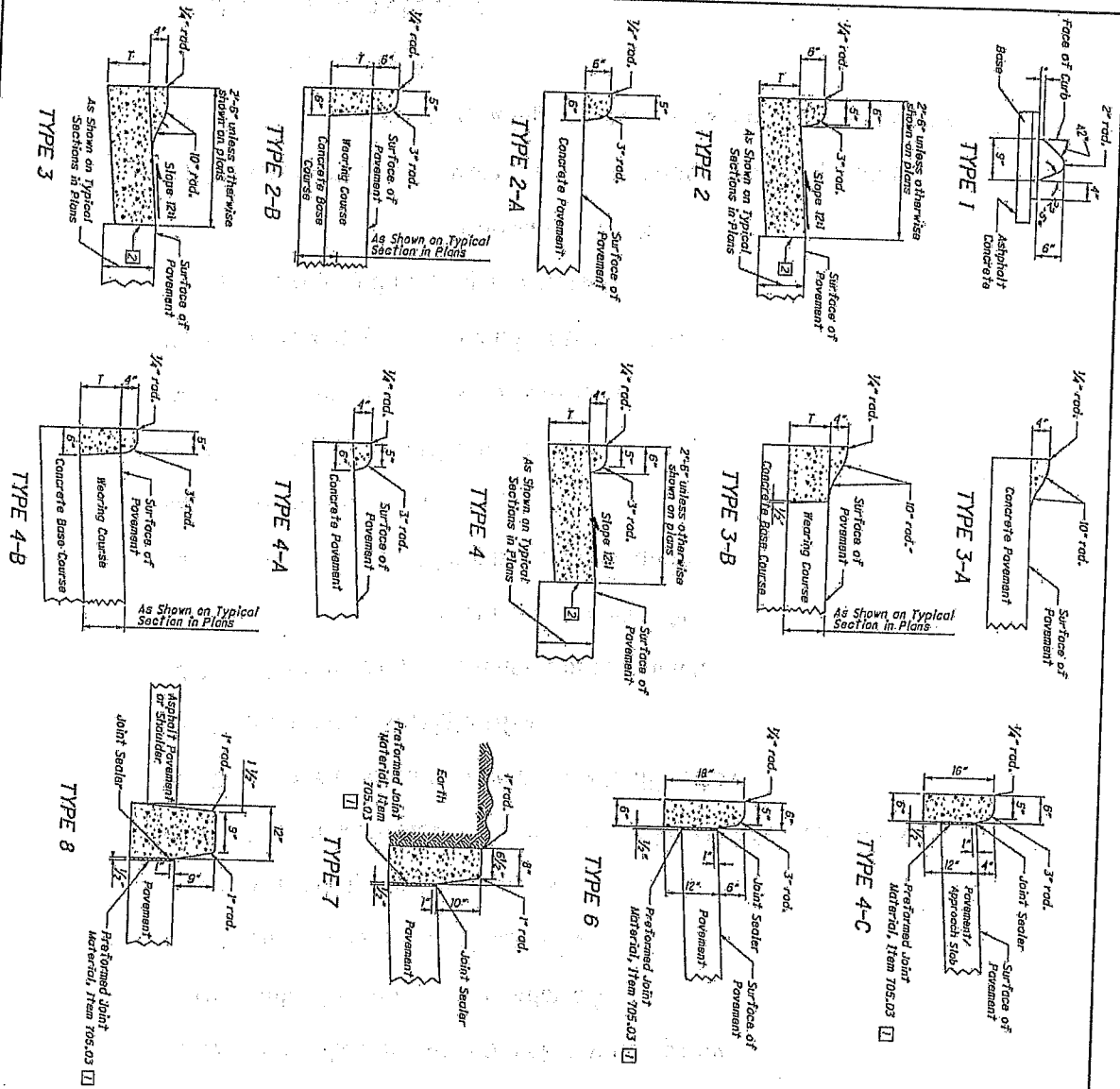
NOTES

GENERAL: The showing details shown here shall govern the construction of driveways unless other notes shown in the project plans.
 The pavement type and thickness shall be specified in the project plans.
 Driveway and mail box approaches shall be constructed with, respectively,
 JOINTS: Impressed joints for portland cement concrete driveways shall be 1/4" minimum width and spaced at 12' maximum.
 In addition to the joints shown here, impressed joints without the use of a jointer shall be placed in portland cement concrete driveways at intervals not to exceed 17' in the portion of the driveway beyond the 10'.

LEGEND

- 1 Unless otherwise shown in the plans.
- 2 Add 3" for each additional Mail Box
- 3 Impressed joint without the use of a jointer for Portland Cement Surface

THIS DRAWING REPLACES BP-4.1 DATED 7-16-04.



NOTES

GENERAL: This drawing shows alternate types of curb that may be used on various types of pavement. The typical section of the project shows the type to be used, also the thickness of the pavement or the edge of the curb and gutter section.

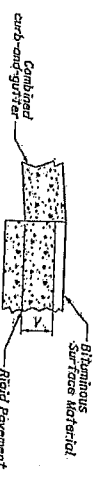
JOINTS: Expansion joints shall extend up to the top of the curb and shall be constructed in the curb and gutter section in such a manner that the joint seal will extend the full width of the curb and into the curb above the flow line of the gutter. Dowel bars shall be used on a 2' wide gutter section of expansion joints and to the surface of the pavement.

GUTTER PLATE THICKNESS: Thickness of gutter plate - 1" shall be 9" unless otherwise shown on the plans.

TOLERANCES: Dimensional tolerances are as follows:
 Curb: 1/8" to 1/4"
 Gutter: 0 to 1/2"

LEGEND

- 1 Expansion joint material and joint sealer are not required for the portion of the curb that is adjacent to the flexible pavement type with materials are required, as detailed, for the full height of rigid pavement and concrete base.
- 2 Joint ribs shall be provided between combined curb-and-gutter and any existing rigid pavements, with the base or back bolts provided at intervals of 5'.
- 3 If the combined curb-and-gutter adjoins a new rigid base or an existing rigid base or pavement that is to be surfaced with bituminous material, a short joint shall be provided. However, the base or back bolts shall be omitted when the pavement is less than 7' between the curb-and-gutter and rigid pavement.



THIS DRAWING REPLACES BP-5.1 DATED 7-29-00.

1 1	1 1	STANDARD ROADWAY CONSTRUCTION DRAWING CONCRETE CURBS AND COMBINED CURB AND GUTTER	OFFICE OF ROADWAY ENGINEERING	STANDARD ENGINEER M. Ruppe	STATE OF OHIO DEPARTMENT OF TRANSPORTATION ADMINISTRATOR Michael Bline	REVISION DATE 7-19-2013
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[Print](#)

Leipsic, OH Code of Ordinances

CHAPTER 94: STREETS AND SIDEWALKS

Section

General Provisions

- 94.01 Conditions precedent to improving streets
- 94.02 Opening permit required
- 94.03 Application and cash deposit
- 94.04 Restoration of pavement
- 94.05 Barriers around excavations
- 94.06 Warning lights
- 94.07 Sidewalk construction by the municipality
- 94.08 Unloading on street or sidewalk
- 94.09 Street or sidewalk obstruction
- 94.10 Materials on street or sidewalk
- 94.11 Duty to keep sidewalks in repair and clean of ice and snow
- 94.12 Ramped curbing for persons with disabilities
- 94.13 Flagpole along right-of-way
- 94.14 Altering or injuring marker or monument

Construction and Repair

- 94.25 Construction and repair may be required
- 94.26 Resolution of necessity
- 94.27 Notice to construct or repair
- 94.28 Assessments of costs against owner
- 94.29 Proceedings may include different owners
- 94.30 Making and levying assessments

Changes in Streets

- 94.40 Change of name, vacating or narrowing streets by petition
- 94.41 Change of name, vacating or narrowing streets without petition
- 94.42 Notice; exception
- 94.43 Publication of notice
- 94.44 Effect of order of vacation

94.45 Effect on public utility easements

94.99 Penalty

Statutory reference:

Assessments generally, see R.C. Chapter 727

Vacation or establishment of streets by court, notice and hearings, see R.C. §§ 723.09 et seq.

GENERAL PROVISIONS**§ 94.01 CONDITIONS PRECEDENT TO IMPROVING STREETS.**

No department of this municipality shall accept, lay out, open, improve, grade, pave, curb or light any street or other way, unless the street or way has been accepted or opened or otherwise has received the legal status of a public street or way prior to the effective date of this code; or unless the street or way corresponds in location and extent with a street or way shown on a recorded plat which has been legally accepted by the Legislative Authority.

Statutory reference:

Power over streets and sidewalks, see R.C. §§ 715.19, 717.01(P), 723.01, 723.011, 723.02, and 729.01

§ 94.02 OPENING PERMIT REQUIRED.

It shall be unlawful for any person, other than an authorized municipal official or the authorized employees or agents of such municipal official, to make any opening in any street, alley, sidewalk, or public way of the municipality unless a permit to make the opening has been obtained prior to commencement of the work.

Penalty, see § 94.99

§ 94.03 APPLICATION AND CASH DEPOSIT.

Each permit for making an opening shall be confined to a single project and shall be issued by the Mayor or other proper municipal officer. Application shall be made on a form prescribed by the Legislative Authority, giving the exact location of the proposed opening, the kind of paving, the area and depth to be excavated, and such other facts as may be provided for. The permit shall be issued only after a cash deposit sufficient to cover the cost of restoration has been posted with the Mayor or other proper municipal officer, conditioned upon prompt and satisfactory refilling of excavations and restoration of all surfaces disturbed.

§ 94.04 RESTORATION OF PAVEMENT.

(A) The opening and restoration of pavement or other surface shall be performed under the direction and to the satisfaction of an authorized municipal official, and in accordance with rules, regulations, and specifications approved by the Legislative Authority.

(B) Upon failure or refusal of the permittee satisfactorily to fill the excavation, restore the surface, and remove all excess materials within the time specified in the permit or where not specified therein, within a

reasonable time after commencement of the work, the municipality may proceed without notice to make such fill and restoration, and the deposit referred to in § 94.03 shall be deemed forfeited. Thereupon, the deposit shall be paid into the Street Repair Fund of the municipality, except such part demanded and paid to the permittee as the difference between the deposit and the charges of the municipality for restoration services performed by it. If the amount of such services performed by the municipality should exceed the amount of the deposit, the Clerk or other proper municipal officer shall proceed to collect the remainder due from such permittee.

§ 94.05 BARRIERS AROUND EXCAVATIONS.

Any person engaged in or employing others in excavating, or opening any street, sidewalk, alley, or other public way shall have the excavation or opening fully barricaded at all times to prevent injury to persons or animals.

Penalty, see § 94.99

§ 94.06 WARNING LIGHTS.

Any person engaged in or employing others in excavating or otherwise in any manner obstructing a portion or all of any street, sidewalk, alley, or other public way, at all times during the night shall install and maintain at least two illuminated warning lamps which shall be securely and conspicuously posted on, at, or near each end of such obstruction or excavation, and if the space involved shall exceed 50 feet in extent, then at least one additional lamp for each added 50 feet or portion thereof excavated or obstructed.

Penalty, see § 94.99

§ 94.07 SIDEWALK CONSTRUCTION BY THE MUNICIPALITY.

It shall be the duty of the engineer of the municipality or, if none exist, another authorized municipal official, to supervise construction or repair of sidewalks within the municipality. He or she shall cause specifications to be prepared for the construction of the various kinds of pavements and transmit the same to the Legislative Authority for approval. When the specifications are approved, the Legislative Authority shall advertise for proposals to do all the work which may be ordered by the municipality in construction and repair of sidewalks, and shall contract therefor, for a period not exceeding one year, with the lowest responsible bidder, who shall furnish good and sufficient sureties for the faithful performance of the work. The Legislative Authority, if it deems advisable, may make separate contracts for the different kinds of work with different parties.

Cross-reference:

Legislative Authority; contracts, see §§ 32.025 et seq.

§ 94.08 UNLOADING ON STREET OR SIDEWALK.

No person shall unload any heavy material in the streets of the municipality, by throwing or letting the same fall upon the pavement of any street, alley, sidewalk, or other public way, without first placing some sufficient protection over the pavement.

Penalty, see § 94.99

§ 94.09 STREET OR SIDEWALK OBSTRUCTION.

(A) No person shall obstruct any street, alley, sidewalk, public ground, or other public way within the municipality by erecting thereon any fence, structure or building, or permitting any fence, structure or building to remain thereon unless authorized by the Legislative Authority or other proper municipal official. Each day that any such fence or building is permitted to remain upon such public way shall be deemed a separate offense.

(R.C. § 5589.01)

(B) Whoever violates this section is guilty of a misdemeanor of the third degree.

(R.C. § 5589.99(A))

Cross-reference:

Driving upon sidewalk prohibited, see § 72.118

§ 94.10 MATERIALS ON STREET OR SIDEWALK.

No person shall encumber any street or sidewalk. No owner, occupant, or person having the care of any building or lot of land bordering on any street or sidewalk shall permit the same to be encumbered with barrels, boxes, cans, articles, or substances of any kind so as to interfere with the free and unobstructed use thereof.

Penalty, see § 94.99

Cross-reference:

Placing injurious materials and litter upon streets or highways prohibited, see § 72.122

§ 94.11 DUTY TO KEEP SIDEWALKS IN REPAIR AND CLEAN OF ICE AND SNOW.

No owner or occupant of lots or lands abutting any sidewalk, curb or gutter shall fail to keep the sidewalks, curbs and gutters in repair and free from snow, ice or any nuisance, and to remove from such sidewalks, curbs or gutters all snow and ice accumulated thereon within a reasonable time, which will ordinarily not exceed 12 hours after any storm during which the snow and ice has accumulated.

(R.C. § 723.011) Penalty, see § 94.99

§ 94.12 RAMPED CURBING FOR PERSONS WITH DISABILITIES.

All new curbs that are authorized by the municipality, and all existing curbs which are part of any reconstruction, shall have a ramp with nonslip surface built into the curb at each pedestrian crosswalk so that the sidewalk and street blend to a common level. These ramps shall be not less than 40 inches wide and shall, insofar as feasible, be constructed in accordance with the standard drawings and specifications for curb ramps of the state Department of Transportation.

(R.C. § 729.12) Penalty, see § 94.99

§ 94.13 FLAGPOLE ALONG RIGHT-OF-WAY.

(A) A property owner in the municipality may install a flagpole between the sidewalk and curb along the right-of-way of any public street or highway adjacent to his or her property. A property owner may also install underground lighting for the display of the flag. Installation of the flagpole and holder shall meet the following specifications:

(1) The flagpole holder shall be embedded in concrete, flush with the sidewalk or sodded area, and possess a cap or cover which shall be used when the holder is not used for the purpose of displaying the American flag;

(2) The holder shall not exceed two inches in diameter and shall be installed not less than one foot from the curb;

(3) Underground lighting for the flagpole shall be situated within a reasonable distance to the holder and meet all underwriters' requirements governing installation. The highest part of the lighting fixture shall at all times be flush with the sidewalk or sodded area in which it is embedded;

(4) At no time shall the flag, flagpole, or holder limit or restrict the view of pedestrian or vehicular traffic, nor shall a flag, flagpole, or holder be installed that comes in physical contact, or is likely to come in physical contact, with overhead wiring.

(B) The municipality may require the issuance of a permit for installation of flagpoles but shall not charge the property owner a permit fee or an inspection fee in excess of \$1 per installation.

(R.C. § 723.012) Penalty, see § 94.99

§ 94.14 ALTERING OR INJURING MARKER OR MONUMENT.

(A) No person shall alter, deface, injure or destroy any marker or monument placed along, upon, or near a public highway by the proper authorities to mark the boundary thereof, or for any other purpose.

(R.C. § 5589.02)

(B) Whoever violates this section is guilty of a minor misdemeanor.

(R.C. § 5589.99(B))

CONSTRUCTION AND REPAIR

§ 94.25 CONSTRUCTION AND REPAIR MAY BE REQUIRED.

In addition to the power conferred upon the municipality under R.C. § 727.01 to construct sidewalks, curbs or gutters and levy an assessment therefor, the Legislative Authority may require the construction or repair of sidewalks, curbs or gutters within the municipality by the owners of lots or lands abutting thereon, and upon the failure of such owners to construct or repair such sidewalks, curbs or gutters within the time prescribed in the resolution adopted under § 94.26, may cause such sidewalks, curbs or gutters to be constructed or repaired and assess the total cost thereof against the lots or land abutting thereon, notwithstanding the provisions of R.C. §§ 727.03 and 727.05.

(R.C. § 729.01)

§ 94.26 RESOLUTION OF NECESSITY.

(A) When it is deemed necessary by the municipality to require the construction or repair of sidewalks, curbs, or gutters within the municipality by the owners of the lots or lands abutting thereon, the Legislative Authority shall cause plans, specifications, and an estimate of the cost of such construction or repair to be prepared, showing the location and dimensions of such sidewalks, curbs or gutters and the specifications for the construction or repair thereof, and to be filed in the office of the Clerk of the Legislative Authority.

(B) After such plans, specifications and estimate of cost have been filed, as provided in this section, the Legislative Authority may declare the necessity for the construction or repair of such sidewalks, curbs or gutters by the adoption of a resolution which shall:

(1) Approve the plans, specifications and estimate of cost of the proposed construction or repair on file as provided by this section;

(2) Describe the lots and lands abutting upon the sidewalks, curbs or gutters to be constructed or repaired by the termini of the improvement or by the street address;

(3) Set forth that such sidewalks, curbs or gutters shall be constructed or repaired by the owners of the lots or lands abutting thereon in accordance with the specifications on file in the office of the Clerk of the Legislative Authority;

(4) Set forth the time within which such sidewalks, curbs or gutters shall be constructed or repaired by the owners of the lots and lands abutting thereon, which shall not be less than 30 days from the date of service of notice under § 94.27 on the owner of the lots or lands;

(5) State that in the event such sidewalks, curbs or gutters are not constructed or repaired by the owners of the lots and lands abutting thereon in accordance with such plans and specifications and within the time prescribed in this resolution, the municipality will so construct or repair such sidewalks, curbs or gutters and assess the costs thereof against the lots and lands abutting thereon.

(R.C. § 729.02)

§ 94.27 NOTICE TO CONSTRUCT OR REPAIR.

Notice of the passage of a resolution of necessity under § 94.26 shall be served by the Clerk of the Legislative Authority, or a person designated by such Clerk, upon the owners of the lots or lands abutting upon the sidewalks, curbs or gutters to be constructed or repaired in the same manner as service of summons in civil cases, or by certified mail addressed to such owner at his or her last known address or to the address to which tax bills are sent, or by a combination of the foregoing methods. If it appears by the return of service or the return of the certified mail notice that one or more of the owners cannot be found, such owners shall be served by publication of the notice once in a newspaper of general circulation within the municipality. The return of the person serving the notice or a certified copy thereof or a returned receipt for notice forwarded by certified mail accepted by the addressee or anyone purporting to act for him or her shall be prima facie evidence of the service of notice under this section. The notice shall also set forth the place where the specifications governing the construction or repair of such sidewalks, curbs or gutters are on file, the time within which the owner of such lot or parcel of land may construct or repair the sidewalks, curbs or gutters, and that in the event the owner does not construct or repair the sidewalks, curbs or gutters in accordance with the specifications and within such time, the municipality will construct or repair such sidewalks, curbs or gutters and assess the costs thereof against the lot or land of the owner.

(R.C. § 729.03)

§ 94.28 ASSESSMENTS OF COSTS AGAINST OWNER.

(A) Upon the expiration of the time within which sidewalks, curbs or gutters shall be constructed or repaired by the owner of the lots or lands abutting thereon as set forth in the resolution adopted under § 94.26, the sidewalks, curbs or gutters not constructed or repaired by the owners of the lots and lands abutting thereon shall be constructed or repaired by the municipality in accordance with the resolution adopted under § 94.26, and the Legislative Authority shall, upon the completion of such construction or repair, assess the costs thereof against the lots or lands abutting thereon.

(B) In anticipation of the collection of the costs of construction or repair of such sidewalks, curbs or gutters from the owners of the lots or lands abutting thereon, the Legislative Authority may provide for the issuance of bonds or notes and the proceeds thereof shall be used to pay for the construction or repair of such sidewalks, curbs or gutters.

(R.C. § 729.04)

§ 94.29 PROCEEDINGS MAY INCLUDE DIFFERENT OWNERS.

In all proceedings pertaining to the construction or repair of sidewalks, curbs or gutters under this subchapter or R.C. §§ 729.01 through 729.08, sidewalks, curbs or gutters upon different streets abutting upon lots or lands owned by different owners may be included in the same resolution, notice, contract, ordinance, or other proceedings.

(R.C. § 729.05)

§ 94.30 MAKING AND LEVYING ASSESSMENTS.

(A) *Estimated assessments.* Upon completion of the construction or repair of sidewalks, curbs or gutters under this subchapter, the total cost of such construction, repair, or installation as defined in division (B) shall be ascertained and reported to the Legislative Authority by its Clerk, and the Legislative Authority shall cause a list of estimated assessments to be prepared. Such list shall include the total cost of such construction, repair, or installation to each lot or land abutting upon such construction, repair, or installation and shall be filed in the office of the Clerk of the Legislative Authority and be available for public inspection.

(R.C. § 729.07)

(B) *Notice of assessment; objection.*

(1) The Legislative Authority shall cause a notice to be published for three consecutive weeks in a newspaper of general circulation in the municipal corporation or as provided in R.C. § 7.16, stating that such list of estimated assessments has been made and is on file in the office of the Clerk of the Legislative Authority for the inspection and examination of persons interested therein.

(2) If any person objects to an assessment on such list, the person shall file the objection in writing with the Clerk of the Legislative Authority within two weeks after the expiration of the notice provided for in division (B)(1) of this section.

(R.C. § 729.08) (Rev. 2012)

(C) *Assessment ordinance.* The Clerk of the Legislative Authority shall deliver the objections received under division (B) of this section to the Legislative Authority. The Legislative Authority shall review the written objections and shall adopt an ordinance levying upon the lots and lands enumerated in the list of estimated assessments the amount set forth on such list with such changes or corrections as the Legislative Authority shall determine to be proper after consideration of the written objections filed under division (B) of this section. Such ordinance shall state the number of annual installments, not exceeding ten, over which the assessments shall be payable and shall establish a period of time during which the assessments shall be payable in cash.

(R.C. § 729.09)

(D) *Assessment proceedings.* The provisions of R.C. §§ 727.26 through 727.43, inclusive, shall apply to and govern the proceedings taken under and the assessments levied under this subchapter. The proceedings taken under this subchapter shall be construed in accordance with the provisions of R.C. § 727.40.

(R.C. § 729.10) (Rev. 1999)

Statutory reference:

Assessments generally, see R.C. Chapter 727

CHANGES IN STREETS

§ 94.40 CHANGE OF NAME, VACATING OR NARROWING STREETS BY PETITION.

The Legislative Authority, on petition by a person owning a lot in the municipality requesting that a street or alley in the immediate vicinity of such lot be vacated or narrowed, or the name thereof changed, upon hearing, and upon being satisfied that there is good cause for such change of name, vacation, or narrowing, that it will not be detrimental to the general interest, and that it should be made, may, by ordinance, declare such street or alley vacated, narrowed or the name thereof changed. The Legislative Authority may include in one ordinance the change of name, vacation, or narrowing of more than one street, avenue or alley. The original ordinance or a certified copy thereof shall be recorded in the official records of the County Recorder.

(R.C. § 723.04) (Rev. 2015)

§ 94.41 CHANGE OF NAME, VACATING OR NARROWING STREETS WITHOUT PETITION.

(A) The Legislative Authority may, when there are two or more streets, avenues or alley of the same name in the municipality, by ordinance and without petition therefor, change the name of any such street, avenue or alley so as to leave only one to be designated by the original name.

(B) When, in the opinion of the Legislative Authority, there is good cause for vacating or narrowing a street or alley, or any part thereof, and that such vacation or narrowing will not be detrimental to the general interest, it may, by ordinance and without petition therefor, vacate or narrow such street or alley or any part thereof. The original ordinance or a certified copy thereof shall be recorded in the official records of the County Recorder.

(R.C. § 723.05) (Rev. 2015)

§ 94.42 NOTICE; EXCEPTION.

Notice of the intention of the Legislative Authority to vacate any street, alley, avenue, or part thereof shall be given as provided in § 94.43, except when written consent to such vacation is filed with the Legislative Authority by the owners of the property abutting the part of the street or alley proposed to be vacated, in which case such notice shall not be required.

(R.C. § 723.06)

§ 94.43 PUBLICATION OF NOTICE.

No street or alley shall be vacated or narrowed unless notice of the pendency and prayer of the petition under R.C. § 723.04 is given by publishing, in a newspaper of general circulation in the municipality, for six consecutive weeks preceding action on such petition, or as provided in R.C. § 7.16 preceding action on the petition. Where no newspaper is of general circulation in the municipality, notice shall be given by posting the notice in three public places therein six weeks preceding such action. Action thereon shall take place within three months after the completion of the notice.

(R.C. § 723.07) (Rev. 2012)

§ 94.44 EFFECT OF ORDER OF VACATION.

The order of the Legislative Authority vacating or narrowing a street or alley which has been dedicated to public use by the proprietor thereof shall, to the extent to which it is vacated or narrowed, operate as a revocation of the acceptance thereof by the Legislative Authority, but the right-of-way and easement therein of any lot owner shall not be impaired by such order.

(R.C. § 723.08)

§ 94.45 EFFECT ON PUBLIC UTILITY EASEMENTS.

When any street, alley or public highway, or a portion thereof, is vacated or narrowed by the municipality pursuant to the provisions of this subchapter or the provisions of R.C. Chapter 723, and the relocation of any conduits, cables, wires, towers, poles, sewer lines, steam lines, pipelines, gas and water lines, tracks, or other equipment or appliances of any railroad or public utility, whether owned privately or by any governmental authority, located on, over or under the portion of the street, alley, or highway affected by such vacation or narrowing, is not required for purposes of the municipality, including urban renewal, any affected railroad or public utility shall be deemed to have a permanent easement in such vacated portion or excess portion of such street, alley or highway for the purpose of maintaining, operating, renewing, reconstructing, and removing such utility facilities and for purposes of access to such facilities.

(R.C. § 723.041)

§ 94.99 PENALTY.

Whoever violates any provisions of this chapter for which another penalty is not already provided shall be subject to the penalty as prescribed in § 10.99.

Village of Leipsic

Concrete Sidewalk Program

BASE BID FOR CONCRETE SIDEWALK

Ref. No.	ODOT Item No.	Item Description	Quantity	Unit	Labor Unit Cost	Material Unit Cost	Total Unit Cost	Subtotal
1	201	Clearing & Grubbing	1	LUMP				
2	202	Signs Removed, Stored and Reinstalled	1	EACH				
3	202	Concrete Walk Removed (Includes Saw Cutting)	25	SY				
4	203	Excavation for Aggregate Base	25	SY				
5	204	Subgrade Compaction	25	SY				
6	411	4" Aggregate Base - Sidewalks	2	CY				
7	608	Curb Ramp, ODOT Type 2, w/ Det. Warning Panel	1	EACH				
8	608	4" Reinforced Concrete Sidewalk	25	SY				
9	614	Maintaining Traffic	1	LUMP				
10	624	Mobilization	1	LUMP				
11	653	Top Soil Furnished & Placed	2	CY				
12	659	Seeding & Mulching	15	SY				
Base Bid Total:							\$	-
Unit Bid Price per SY: (Base Bid Bid Total, less Curb Ramp & Warning Pad divided by 25)							\$	-

ALTERNATE BID FOR CONCRETE DRIVE APRON & CURB

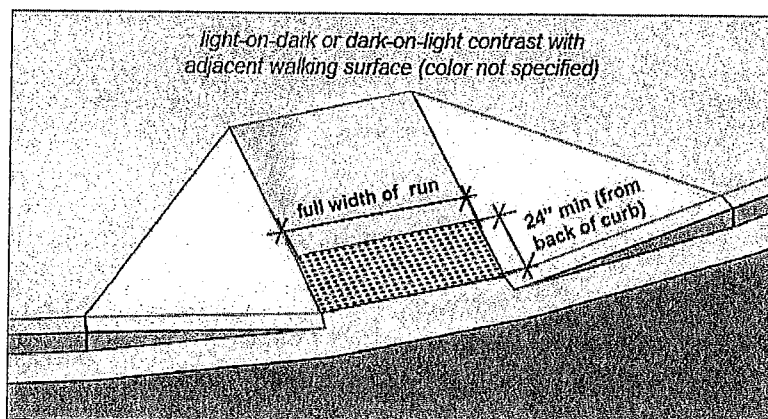
1	201	Clearing & Grubbing	1	LUMP				
2	202	Concrete Drive Removed (Includes Saw Cutting)	15	SY				
3	202	Asphalt Drive Apron Removed (Includes Saw Cutting)	15	SY				
4	202	Aggregate Drive Apron Removed	15	SY				
5	203	Excavation for Aggregate Base	15	SY				
6	204	Subgrade Compaction	15	SY				
7	411	4" Aggregate Base - Concrete Drives	5	CY				
8	608	6" Reinforced Concrete Drive Apron	15	SY				
9	609	Combination Curb & Gutter (24"), Type 2	60	LF				
10	614	Maintaining Traffic	1	LUMP				
11	624	Mobilization	1	LUMP				
12	653	Top Soil Furnished & Placed	1	CY				
13	659	Seeding & Mulching	5	SY				
Alternate Bid Total:							\$	-
Unit Bid Price per SY: (Base Bid Bid Total divided by 15)							\$	-

Detectable warnings are required on curb ramps at transit facilities covered by DOT's Standards (facilities used by state and local governments to provide designated public transportation services, such as rail stations and bus stations). They are not required at non-transit facilities subject to DOJ's Standards. DOT also requires detectable warnings on curb ramps in projects funded by the Federal Highway Administration. New guidelines the Board is developing for public rights-of-way will address requirements for detectable warnings at curb ramps and other transitions along public streets and sidewalks where hazards to people with vision impairments are greater.

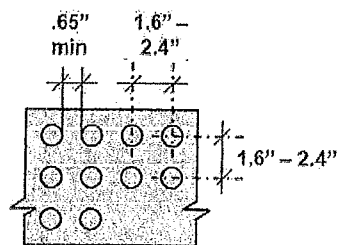


Where detectable warnings are required (or provided anyway) they must meet specifications for size, spacing, and contrast. These detailed criteria provide a distinctive texture intended to have a uniform meaning in alerting persons to the approach to vehicular areas (as well as drop-offs along rail station platforms).

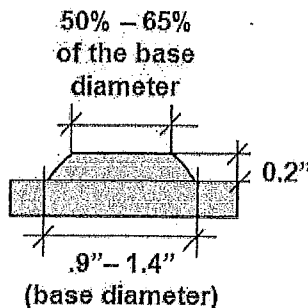
Detectable Warnings on Curb Ramps



Dome Spacing



Dome Size



Common Questions

Ramps

Is there a limit on the total length (number of runs) a ramp may have?

No, the Standards limit the rise of each run (30" max.), but not the overall length of ramps comprised of multiple runs. Since the usability of ramps decreases with length, considerably long ramp systems should be avoided where possible. (Ramps in play areas are limited to a 12" max. rise).

Are curved or circular ramps allowed?

Ramps without level landings at changes in direction typically will not meet the Standards due to resulting compound slopes. This includes most circular or curved ramps, unless the radius is large enough so that the cross slope is compliant and compound slopes are avoided. Otherwise, the curvature and slope result in uneven surfaces that makes wheelchair maneuvering difficult because not all wheels rest evenly on the surface.

Can ramps be portable or provided after construction as an adaptation?



All required ramps and curb ramps must be permanent and installed at the time of construction or alteration with few exceptions. Ramps can be provided after construction only to provide access to raised work stations in courtrooms (sufficient space must be provided in design to facilitate installation). Only ramps serving temporary structures can be temporary or portable.

Is there a maximum distance between ramp handrails?

No, only a minimum (36") is required between handrails. The Standards do not require center or intermediate handrails on wide ramps.

Can handrails overlap ramp landings?

Along runs and landings, the minimum clear width must be measured between the leading edge of handrails. This also applies to landings required to be at least 60" by 60" where ramps change directions so that the minimum landing area remains clear.

Can handrail extensions wrap or turn?

Extensions are not required for continuous handrails along switchback or dogleg ramps or at aisle ramps serving seating in assembly areas. In alterations where the required extension would project hazardously into circulation paths, they can turn, be shorter, or avoided. Otherwise, handrail extensions must be in same direction as the ramp run.

Can handrails be mounted to guard rails?

Yes, handrails can be installed on guard rails if all applicable requirements are met.

Curb Ramps

Why is a top landing required at curb ramps if side flares are provided?

A landing at least 36" long at the top of curb ramps provides room to approach or exit ramps and turn without encountering compound slopes of flared sides. Side flares are intended mainly to prevent tripping hazards. They are not designed to accommodate wheelchair maneuvering except in alterations where space constraints preclude a compliant top landing.

Are side flares required at curb ramps?

The ADA Standards do not require that curb ramps have side flares, but limit the slope (1:10 max.) where they are provided. Side flares are advisable where pedestrian traffic may cross runs to prevent tripping hazards. Side flares are essential in alterations when space for a top landing (36" deep min.) is not available; in this instance, side flares (1:12 max. slope) are necessary to accommodate wheelchair maneuvering that will partially occur at flares in the absence of full landing space at the top unless a parallel-type curb ramp is provided.

Are detectable warnings required on curb ramps or at hazardous vehicular areas?

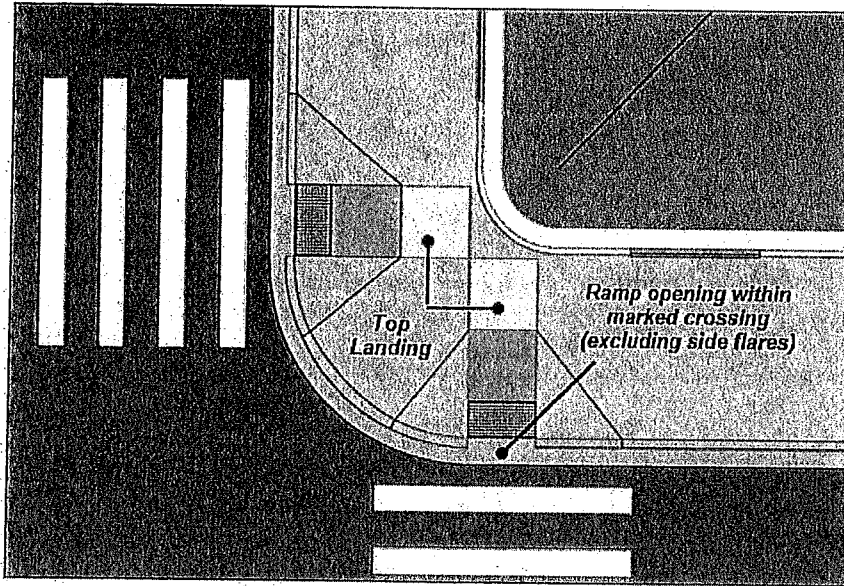
Under the ADA Standards, which apply primarily to facilities located on sites, detectable warnings are required at transit facilities (at curb ramps and along open drop-offs of rail station boarding platforms). Specifically, the curb ramp requirements apply only to public transportation facilities covered by DOT's ADA Standards. Curb ramps at all other facilities are not required to have detectable warnings. New guidelines for public rights-of-ways issued by the Board will address detectable warnings on curb ramps and other transitions along public streets and sidewalks. In addition, DOT requires detectable warnings on curb ramps in projects funded by the Federal Highway Administration.

What types of facilities are covered by DOT's ADA Standards (and are required to have detectable warnings on curb ramps)?

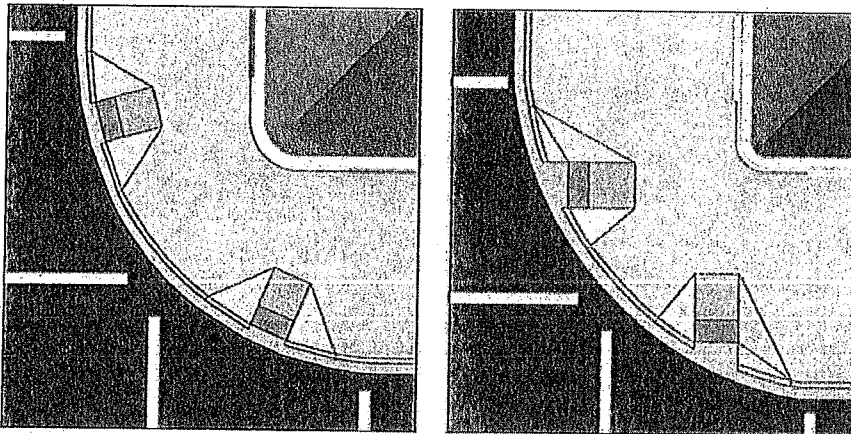
DOT's ADA Standards apply to facilities used to provide "designated public transportation." This includes transportation services state or local governments provide the general public on a regular and continuing basis by bus, rail, or other conveyance (excluding aircraft and public school transportation). DOT's ADA Standards also apply to intercity and commuter rail stations. Curb ramp detectable warnings are required only at these facilities. Detectable warnings are required at rail station boarding platforms with open drop-offs at any transit facility, including private sector transit facilities subject to DOJ's ADA Standards.

Why are detectable warnings no longer required for all curb ramps or at hazardous vehicular areas?

In the last update, the ADA Standards were revised to focus more clearly on facilities located on sites in recognition of separate criteria the Access Board is developing for public rights-of-ways. The public rights-of-way guidelines will address requirements for detectable warnings due to hazards to people with vision impairments along public streets and sidewalks. At facilities located on sites, various measures can help reduce hazards, including reduced traffic speeds, marked crossings with pedestrian right-of-way, and speed-bumps, and other optional traffic calming measures.

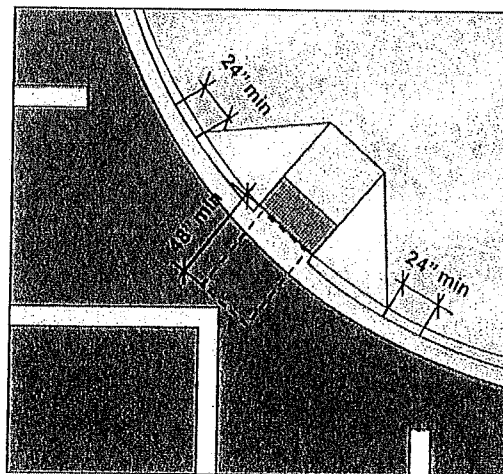


Curb ramps must be oriented so that the grade break is perpendicular to the curb ramp run to ensure a smooth transition to streets, including at corners with a wide radius. The curb ramp opening can be aligned with the curb line (left) or more directionally oriented to the crosswalk (right).



Diagonal Curb Ramps [§406.6]

If curb ramps are placed diagonally at an intersection, it is important that clear space 48" long min. is available at the bottom that is outside active vehicle traffic lanes and is located within marked crossings, where provided. A segment of curb at least 24" long beyond flares must be provided on both sides of curb ramps with side flares within marked crossings. This curb segment provides an orienting cue at crossings for people with vision impairments.



642.2 Sidewalk Ramp and Curb Ramp Design Criteria

From Engineering Policy Guide

Additional Information

ADA Standards Section 4.8.5 (<http://www.ada.gov/reg3a.html#Anchor-19425>)

Sec 608 (http://www.modot.org/business/standards_and_specs/SpecbookEPG.pdf#page=9)

Standard Plan 608.40

(http://www.modot.org/business/standards_and_specs/Online%20Standard%20Plans/60840.pdf)

Standard Plan 608.50

(http://www.modot.org/business/standards_and_specs/Online%20Standard%20Plans/60850.pdf)

Standard Plan 620.00

(http://www.modot.mo.gov/business/standards_and_specs/documents/62000.pdf)

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 - 2.4 642.2.2.4 Landings
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- 3 642.2.3 Detectable Warnings
- 4 642.2.4 Side Streets and Driveway Crossings
- 5 642.2.5 Crosswalks

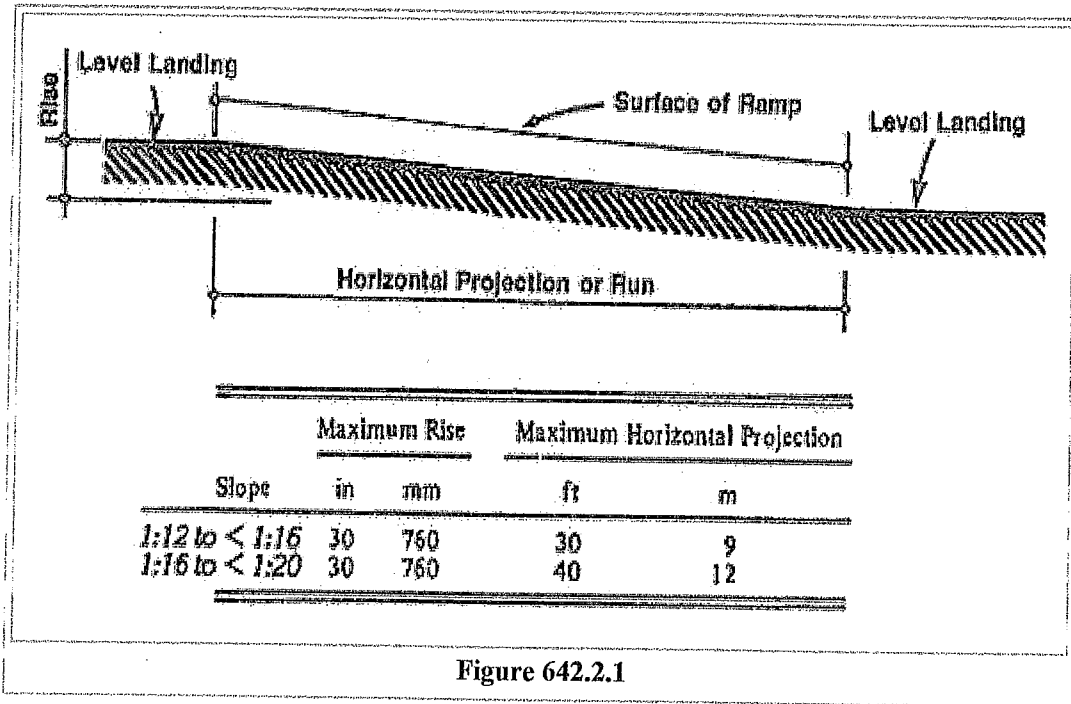
642.2.1 Slope and Rise of Sidewalk Ramps

When the running slope, or grade, of a sidewalk exceeds 5% it is a ramp. If the sidewalk is adjacent to the street or separated by a narrow planting strip, the sidewalk grade may be equal to the grade of the street and not be considered a ramp. Ramps typically occur on an accessible route leading to a facility or otherwise separated from the street.

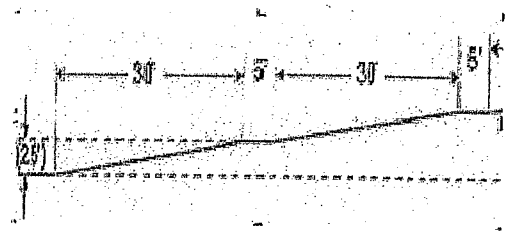
The cross slope for all ramps is to be 1%, but a maximum of 2.0% is allowed by ADA standards. Although the maximum running slope of a ramp in new construction is 1V:12H (8.33%), as discussed before, all sidewalks are to be designed with the least running slope possible. In an alteration project if it

is technically infeasible to meet the running slope requirement, every effort should be made to flatten the slope as much as possible and provide landings where necessary. Clear documentation of any exceptions should be kept in the project file.

The maximum rise in any run will be 30 in. Examples of various slopes and ramp lengths are shown in Figure 642.2.1.



A landing will be located at the top and bottom of all ramps and between segments that have a 30 in. rise. The landing will be at least the width of the ramp with a minimum length of 60 in. If a turn is required the landing must be 5 ft. x 5 ft. For example, a segment with a running slope of 1V:12H, or 8.33% will require a 5 ft. x 5 ft. landing every 30 ft. if it is part of a switchback access route.



A vertical rise greater than 6 in. will require a handrail. Handrails must be compliant with ADA standards, Section 4.8.5 (<http://www.ada.gov/reg3a.html#Anchor-19425>)

Edge protection will be provided on ramps and landings with drop-offs and shall have curbs, walls, railings, or projecting surfaces that prevent people from slipping off the ramp. Curbs shall have a minimum height of 2 in.

642.2.2 Curb Ramps

Curb ramps will be provided wherever a sidewalk crosses a curb. Curb ramps should not be installed where there are no sidewalks as they may mislead a person with disabilities to an area where an accessible pedestrian pathway is not provided. However, if there is a commitment to install sidewalks in the near future, approximately 1 year or less, curb ramps should be installed with a current project if it

results in more efficient construction and lower cost. Curb ramps should be installed at each end of an accessible route to allow access onto the route.

The design and construction of curb ramps require thought and planning. The curb ramp must be designed to meet the existing topographical and physical constraints, and the requirements for curb ramp slope, cross slope, landings and connection to the street.

Curb and sidewalk ramps will be designed in accordance with the standard plans, or varied to fit the needs at a particular location. Situations that do not fit the standard plans are to be identified on a field check. If a particular curb ramp differs from the standard plans, the ramp will be detailed on the plans. The exact location of the curb ramps and crosswalks for intersections will be shown on the plans.

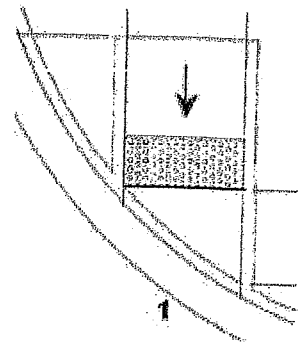
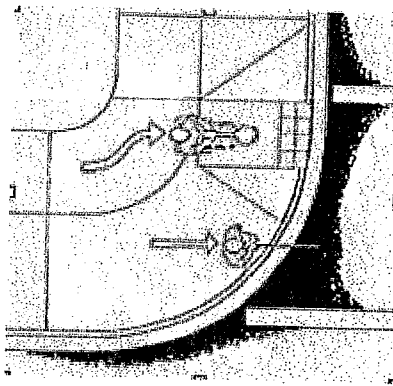
The following criteria apply to all curb ramp situations:

- Width - curb ramps in new construction will have a clear width of 5 ft., exclusive of flared sides. In an alteration, the width will be the same as the adjoining sidewalk or a minimum of 4 ft.
- Cross slope - The cross slope on all curb ramps will be a minimum of 1%, up to 2.0% maximum.
- Running slope - The least possible running slope will be used for all ramps. The minimum running slope for drainage purposes is 1%. In new construction, the maximum slope allowed by ADAAG is 8.33% (1V:12H). Consideration should be given to construction accuracy and future settlement.
- Grade breaks - Grade breaks at the top and bottom of perpendicular curb ramps shall be perpendicular to the direction of ramp run. Grade breaks shall not be permitted on the surface of curb ramps, blended transitions and landings within the pedestrian access route. Surface slopes that meet at grade breaks shall be flush.
- Landings - Curb ramps: A minimum 5 ft. x 5 ft. level landing with a minimum of 1% and no more than 2.0% cross slope in any direction will be provided wherever two sidewalks cross, or a turn may be required. Perpendicular curb ramps may require a pedestrian to choose a direction from a landing at the top of the ramp. Parallel curb ramps require a landing at the bottom so that a pedestrian can turn to enter the street. A diagonal ramp requires a landing at the top and a 4 ft. x 4 ft. clear space at the bottom protected within the crosswalk markings.
- Detectable warnings, or truncated domes - Detectable warnings must be placed at all public streets and signalized intersections. They will be located at the bottom of each ramp a maximum of 5 ft from the curb face, or 6 to 8 in. from the curb face if the distance to the bottom of the ramp is greater than 5 ft. Additional information may be found in EPG 642.2.3 Detectable Warnings.
- Islands and medians - Raised islands and medians in crosswalks will be cut through level with the street and include a level passing space of 5 ft. x 5 ft. The cut through width will meet the requirement of an accessible route. Median cut through length is to be 4 ft. min. Detectable warning will be provided at each entrance to the street.
- Steps - Sidewalk ramps will be provided at locations where steps occur, such as at the ends of bridges having sidewalks across the bridge or at pedestrian grade separations.

- Obstacles - No obstacles (grates, utility covers, etc.) should be within a ramp. If they cannot be avoided, they will have stable, firm and slip resistant surfaces, have flush transitions, and meet the change in level requirements. Storm drain inlets are to be designed outside of the ramp area, but located so as to minimize water runoff or pooling of water at the bottom of ramps.
- Parking - Curb ramps will be located or protected to prevent their obstruction by parked vehicles.
- Crosswalk markings - Curb ramps at marked crosswalks will be wholly contained within the markings, excluding any flared sides. The minimum crosswalk is 6 ft.
- Pay items - The designer will estimate the square yardage for curb ramp and include it with the estimate for sidewalks and the quantity will be shown on the 2B sheet(s). A separate pay item is included for detectable warnings since they are not required on all ramps, and in many cases, are required to be retrofitted on existing curb ramps.
- See the Sec 608
http://www.modot.org/business/standards_and_specs/SpecbookEPG.pdf#page=9 , Standard Plan 608.50
http://www.modot.org/business/standards_and_specs/Online%20Standard%20Plans/60850.pdf
 and EPG 642.1.4 for additional information.

642.2.2.1 Curb Ramp Alignment

The accessible route should be perpendicular to the curb being crossed to provide a level cut for wheelchairs and directional cues for the visually impaired. By placing curb ramps perpendicular to the curb, grade changes are at right angles that will allow all four wheels of a wheelchair to be in contact with the ground at all times. Curb ramps located at the tangent point provide the shortest pedestrian crossing. Alignment of curb ramps, crosswalks and raised islands is an important consideration in providing a safe crossing for the visually impaired.



For large radii, it is often not possible to both place curb ramps perpendicular to the curb and in-line with the pedestrian crossing. Ramps may be set back from the curb to provide a grade break that is perpendicular to the ramp slope and a landing or blended transition will continue to the curb. If the bottom of the ramp is more than 5 ft. from the curb, detectable warnings are placed at the back of the curbline. Parallel ramps are often useful in this situation.

642.2.2.2 Flares on Ramps

If a curb ramp is located where pedestrians may walk across the ramp, or the ramp is not protected by handrails, guardrails, or a permanent obstacle, and the landing or the sidewalk at the top of the ramp is 4 ft. or wider, the flared sides of the ramp will have a maximum slope of 1V:10H (10%). If, due to a technical infeasibility, the sidewalk is narrower than 4 ft., the maximum slope will be 1V:12H (8.33%). The sidewalk width, X in Figure 642.2.2.2, must be at least 3 ft. A parallel ramp may be more appropriate in some situations.

Vertical curbs or flares greater than 1V:10H are allowed where the pedestrian path will not cross the ramp. Refer to Figure 642.2.2.2. Handrails are not required on curb ramps.

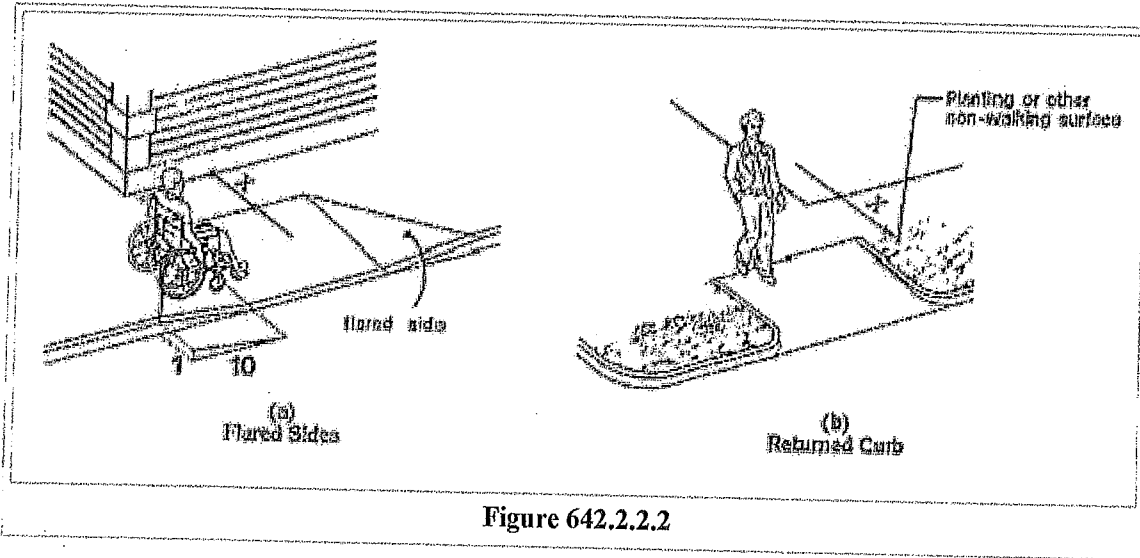


Figure 642.2.2.2

642.2.2.3 Gutters

The gutter slope is parallel to the ramp and perpendicular to the curb. Because the slopes of adjacent gutters and streets significantly affect the overall accessibility of curb ramps, the rate of change of grade between the gutter and the ramp must not exceed 13% over a 2 ft. interval. The cross slope of the road or gutter surface immediately adjacent to the curb ramp must not exceed 5%.

The transition between the ramp and gutter is to be smooth and flush. For new construction, transitions from curb ramps to sidewalks, gutters or streets must be flush and free of abrupt changes. For existing locations where a smooth transition cannot be achieved, changes in level between $\frac{1}{4}$ in. and $\frac{1}{2}$ in. must be beveled with a slope no greater than 1V:2H, and changes in level greater than $\frac{1}{2}$ in. need to be removed or ramped. Maximum slopes adjoining a curb ramp must not exceed 1V:20H (5%).

It is important to ensure that water does not puddle at the curb opening. Where drainage crosses the opening and may enter the ramp, or landing area, a maximum 8.33% slope may begin at the face of the curb, resulting a $\frac{1}{2}$ in. rise at the back of the curb in a 6 in. curb opening.

642.2.2.4 Landings

Landings at curb ramps allow people with mobility impairments to move completely off of the ramp and onto the sidewalk. They provide a level area for resting, turning, or reaching pedestrian signal buttons. The slope for all landings must be between a minimum of 1% for drainage and a maximum of 2.0% for pedestrian stability in all directions.

A minimum 5 ft. x 5 ft. landing must be provided wherever two sidewalks cross, or a turn may be required. Perpendicular curb ramps may require a pedestrian to choose a direction from a landing at the top of the ramp. Parallel curb ramps require a landing at the bottom so that a pedestrian can turn to enter the street. A diagonal ramp requires a landing at the top and a 4 ft. x 4 ft. clear space at the bottom protected within the crosswalk markings.

For retrofit projects, where geometric or right of way restrictions exist, the landing width may be reduced to 4 ft. MoDOT policy will be 5 ft. x 5 ft. to coincide with sidewalk widths and passing space. If landings are reduced below 4 ft., the reasons need to be documented. Please note that requirement for ramps differ from those of curb ramps in that ramps must have the same width as the approaching sidewalk and a minimum length of 5 ft.

642.2.2.5 Types of Curb Ramps and Design Applications

Curb ramps at intersections need to be designed on an individual basis. The location and type of the ramps are to be shown on the plans to ensure all details of the ramps have been taken into consideration and are constructable to MoDOT standards. Determining pedestrian pathways early in the design process is essential. The best ramp configuration for a given location may impact the location of utilities, traffic signals, light poles, storm drainage and other roadside features. It is recommended that the sidewalk and ramps be laid out early in the preliminary plans and in coordination with other roadside features.

There are a variety of curb ramp types. The pattern to be used depends on the location, type of street and existing design constraints. Curb ramps are normally categorized by their position relative to the curb line. The three most common configurations are perpendicular, parallel and diagonal. See EPG 642.3 Curb Ramp Applications and Standard Plan 608.50 (http://www.modot.org/business/standards_and_specs/Online%20Standard%20Plans/60850.pdf) for more details.

642.2.3 Detectable Warnings

ADAAG defines a detectable warning as "a standardized surface feature built in or applied to walking surfaces or other elements to warn visually impaired people of hazards on a circulation path." Detectable warnings are provided on curb ramps at side streets and signalized driveways, such as at a mall or major outlet/discount store. Truncated domes are never to be installed on ramps for residential driveways.

Detectable warnings on walking surfaces are required to be truncated domes with a diameter of 0.9 in., a height of 0.2 in., and a center to center spacing of 1.65 in. to 2.35 in. in each direction. The truncated dome panel is to contrast visually with adjoining surfaces, either light-on-dark or dark-on-light. (Refer to

Standard Plan 608.50 (http://www.modot.org/business/standards_and_specs/documents/60850.pdf). The preferred color contrast is red for concrete and yellow for asphalt surfaces.

The truncated dome surface area is a minimum length of 2 ft. covering the entire width of the ramp or curb opening, excluding the flare sides. The width of the truncated domes may be shortened by 2-3 in. on each side if recommended by the manufacturer for durability. The truncated domes are aligned on a square grid in the predominate direction of pedestrian travel to permit wheelchair wheels to roll between the domes. They are to be placed at the bottom of a ramp perpendicular to the path of travel, and parallel to the grade break, or 6-8 in. from the front of the curb depending on the type and location of the curb ramp.

On large curb radii, one edge of the truncated dome surface area may be placed at the back of the curb to minimize the distance of the other edge from the curb. If either corner of the detectable warning is more than 5 ft. from the back of the curb the detectable warnings should be placed in a radial pattern 6-8 in. from the face of the curb.

Detectable warnings are also provided at cut-throughs in islands and medians. Where islands or medians must be less than 4 feet in length, the detectable warnings are to extend across the full length of the cut through, island or median. At islands and medians the detectable warnings are to be placed at the curblines.

Where truncated domes are placed at the bottom of a ramp, the path between the domes shall be parallel to the path of travel on the ramp. At a blended transition or on a landing, the direction of the path between the domes may vary.

Where a sidewalk crosses a railroad track, the detectable warning surface shall be located so that the edge nearest the rail crossing is 6 ft. minimum and 15 ft. maximum from the centerline of the nearest rail. The rows of truncated domes in a detectable warning surface shall be aligned to be parallel with the direction of wheelchair travel.

642.2.4 Side Streets and Driveway Crossings

A defined walkway is required across all driveways. The defined walkway will consist of a paved area 4 ft. wide, at minimum, with a cross slope of 1% (2.0% maximum) to meet ADA standards. It does not have to be marked, but will provide an accessible path between the adjoining sidewalks or ramps. The running slope will 5% or less, or at the same grade as the roadway. Where possible, keep the entrance and sidewalk at the same grade, eliminating the need for ramps. When designing these pathways across entrances, it will be necessary to adjust the approaches to the defined pathway to prevent bottoming out of vehicles transversing the defined pathway.

642.2.5 Crosswalks

Crosswalks are a critical part of the pedestrian network. A crosswalk is defined as "the portion of a roadway designated for pedestrians to use in crossing the street" and may be either marked or unmarked. However, marked crosswalks are the most effective and are recommended where there are traffic signals. Marked crosswalks provide warning to motorists and assist pedestrians, especially pedestrians

with low vision, in maneuvering across the street. Marked crossings are delineated in materials or markings that provide a visual contrast with the surface of the street and meet, at minimum, the dimensions shown on Standard Plan 620.00 (http://www.modot.mo.gov/business/standards_and_specs/documents/62000.pdf).

Crosswalk placement requires balancing several goals that sometimes compete.

- Curb ramps must be placed within the crosswalk.
- When a diagonal curb ramp is at an intersection, a 4 ft. x 4 ft. clear landing space is provided within the crosswalk to allow persons in wheelchairs enough room to maneuver into the desired direction while maintaining a safe distance from moving vehicles.
- The crosswalk lengths are kept to a minimum to limit the time to cross the roadway and the time people are in the crosswalk.
- Setback of crosswalks are to be kept to a minimum to reduce out-of-direction travel and to provide good sight lines between pedestrians and motorists.

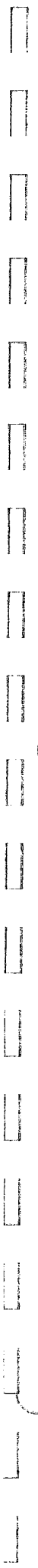
In some situations, signage may be warranted in addition to a marked crosswalk. For example, at non-signalized, high speed intersections, drivers do not recognize a marked crosswalk quickly enough to react to pedestrians in the crosswalk. In-roadway lights or pedestrian crosswalk signs are treatment that may be used to increase pedestrian safety. Another case is at signalized intersections with islands and free right turns where pedestrian crosswalk signs along with the marked crosswalk remind motorists to yield to pedestrians.

Retrieved from "http://epg.modot.org/index.php?title=642.2_Sidewalk_Ramp_and_Curb_Ramp_Design_Criteria"
Category: 642 Pedestrian Facilities

- This page was last modified on 9 June 2016, at 09:38.

Sidewalk Survey

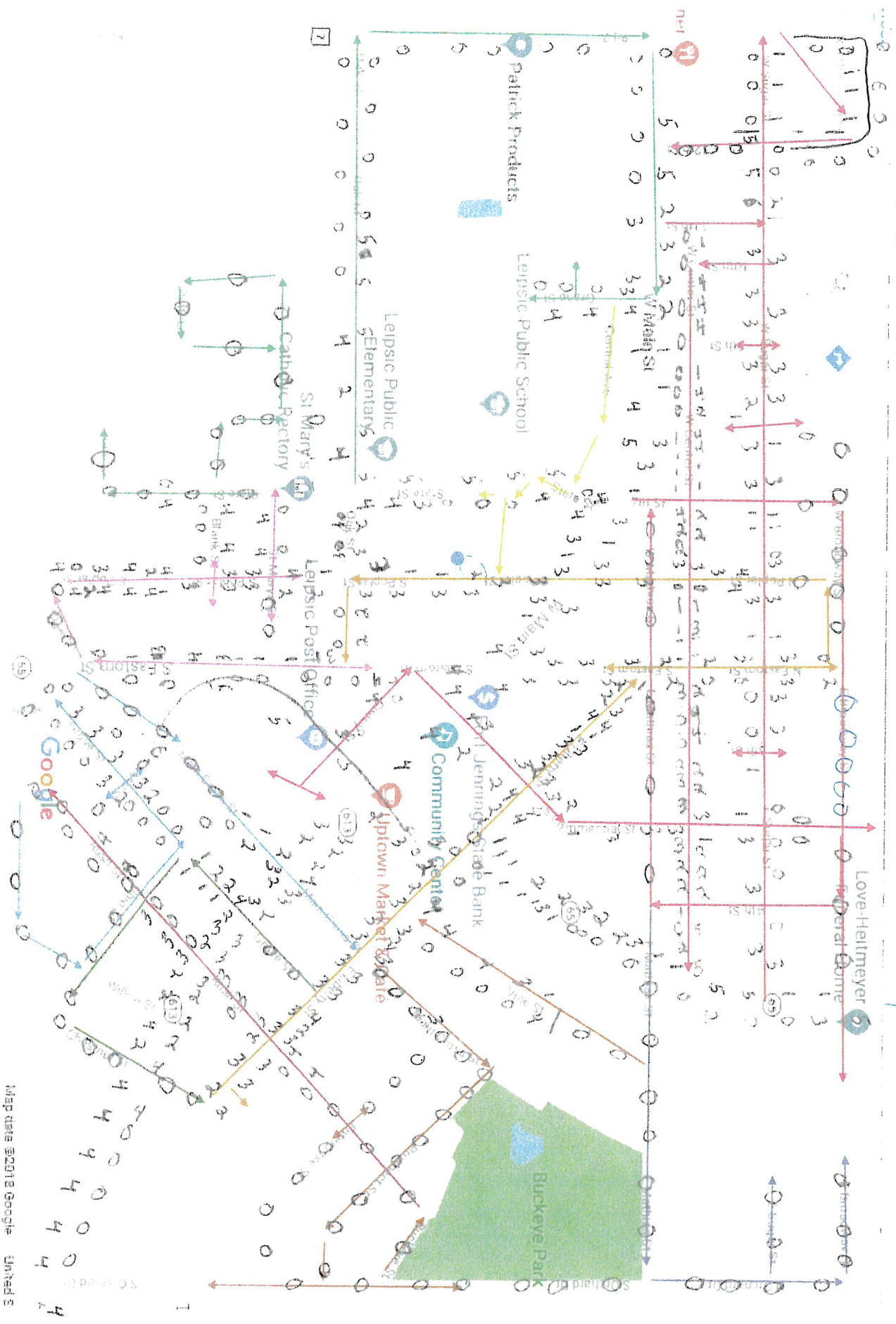
A sidewalk survey would be added in this place.



No. 5 benches
 News sidewalks
 5

Side walk survey

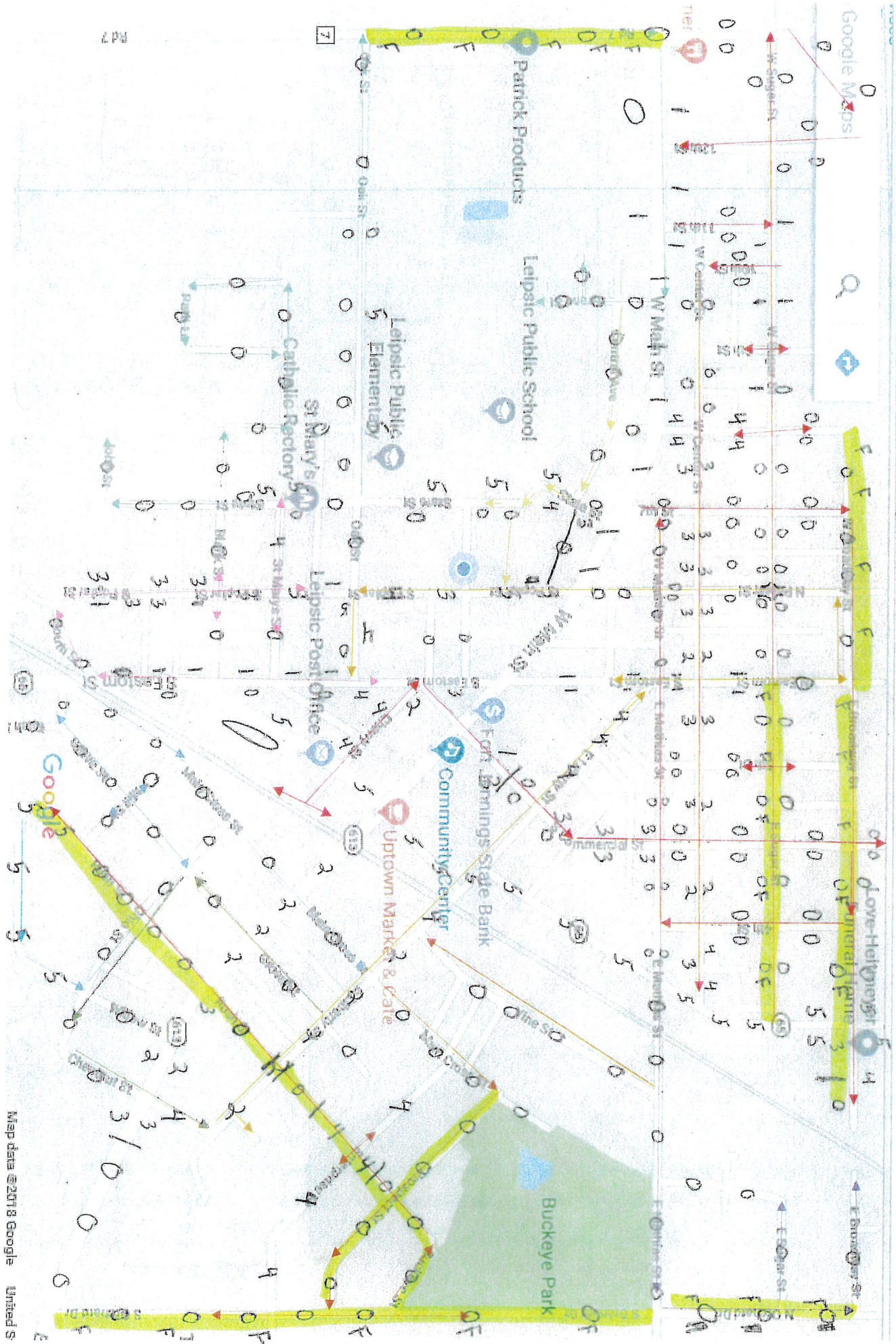
12-1-18



11/30/2018
Curbside Review

No curbs 0
New curbs 5

11/30/18



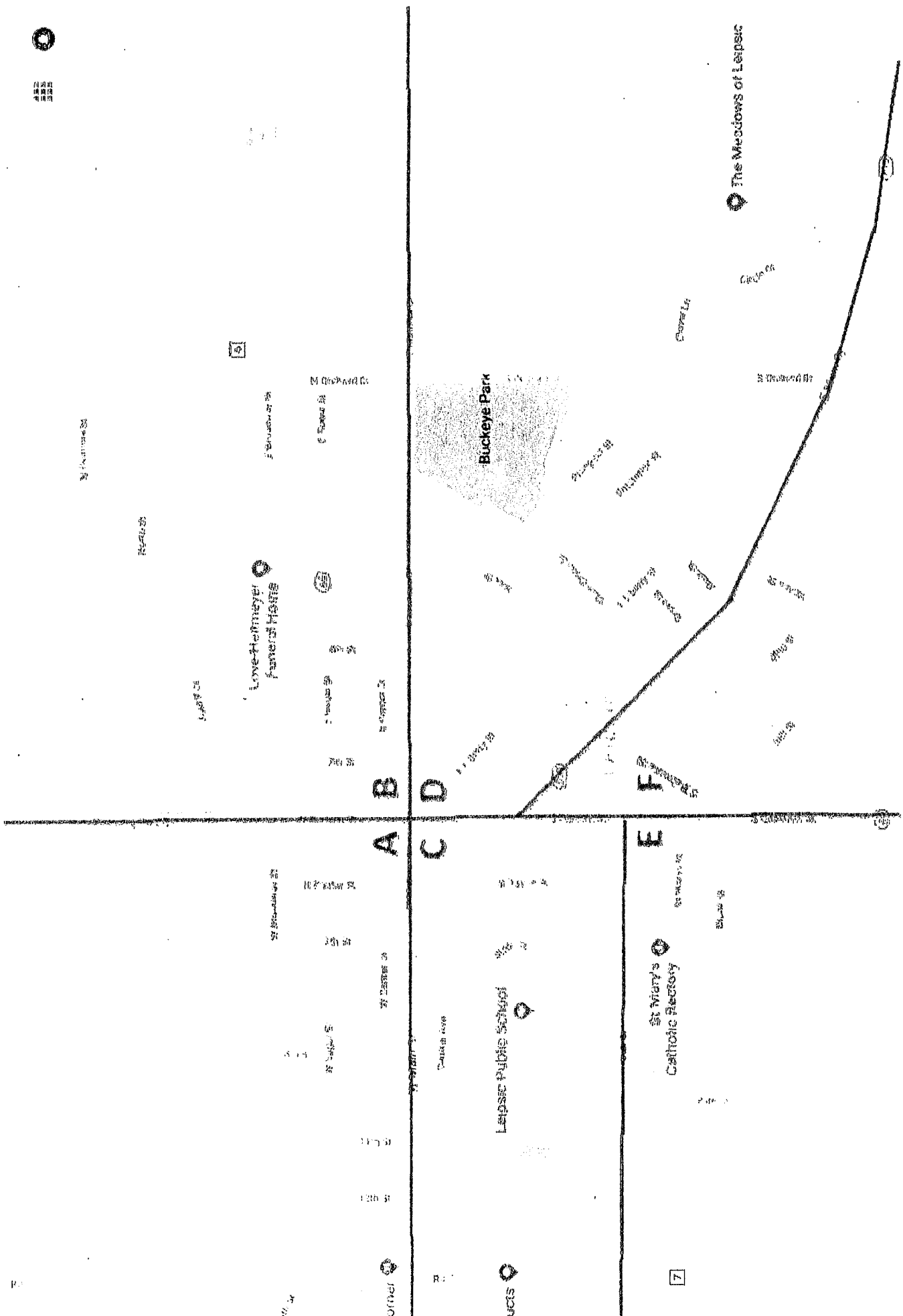
7

Maps

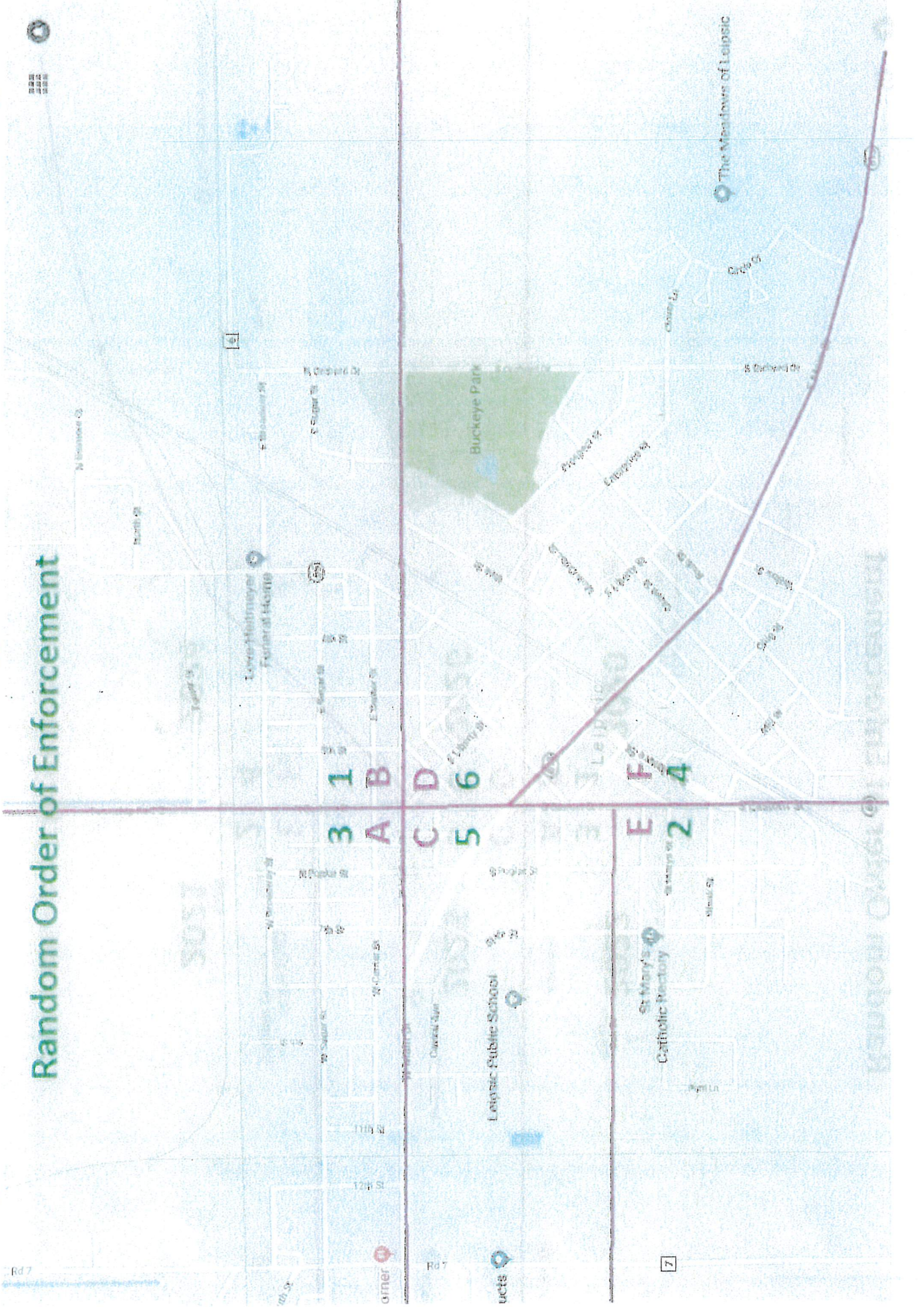
The sidewalk project requires several needed maps. These maps will show the order for enforcement. They will identify places where the Village owns public property, where it owns vacant property and where alleys are located. A map will be needed to reflect existing public sidewalks and more maps will be needed to show trip hazards, watch areas and private grounds with no sidewalks.



11 11 11 11 11



Random Order of Enforcement



Rd 7

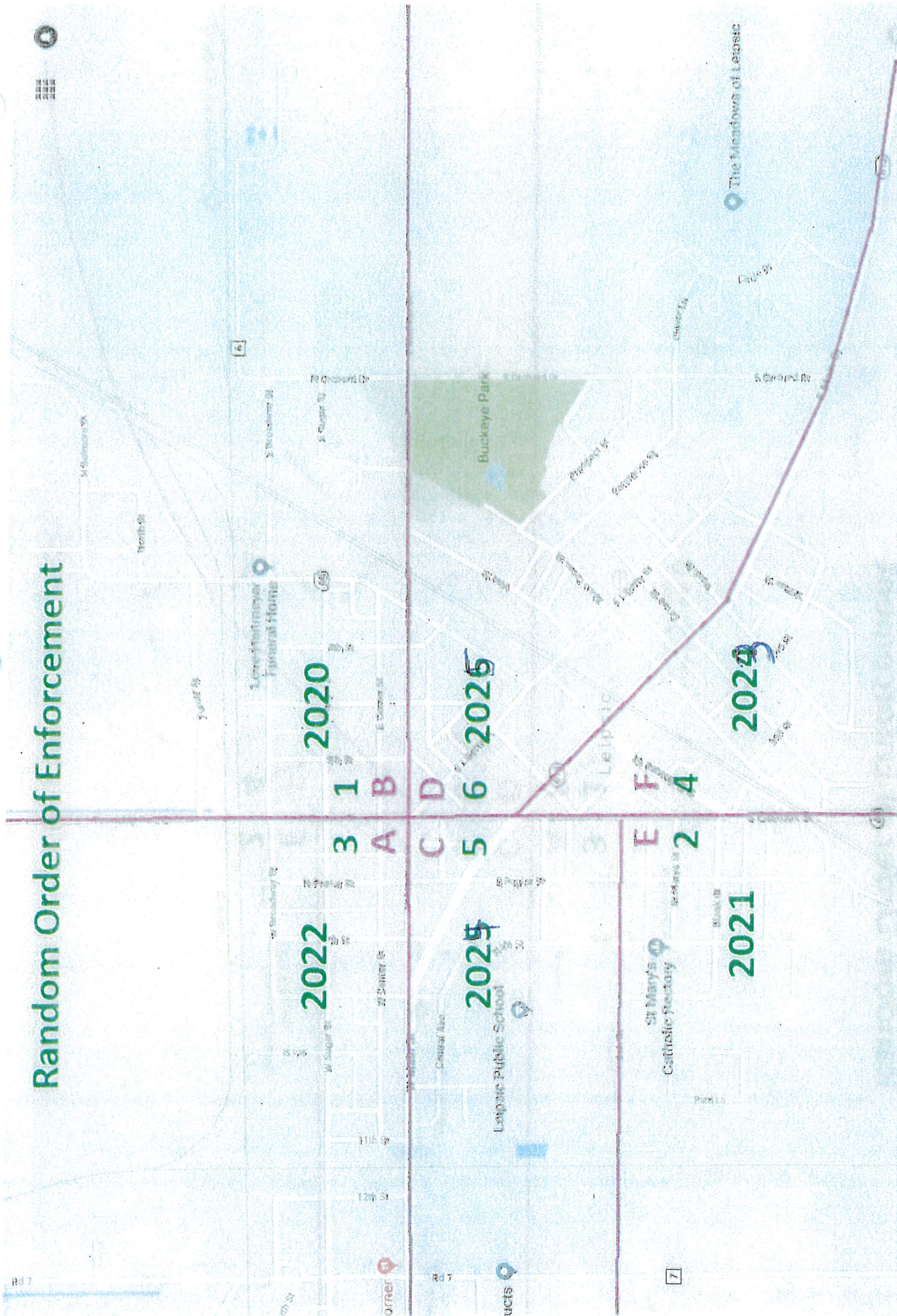
Rd 7

7

Other

ucl's

Random Order of Enforcement



D

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A

C

E

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F

PHO-TEC
Coating Company

65

0.5

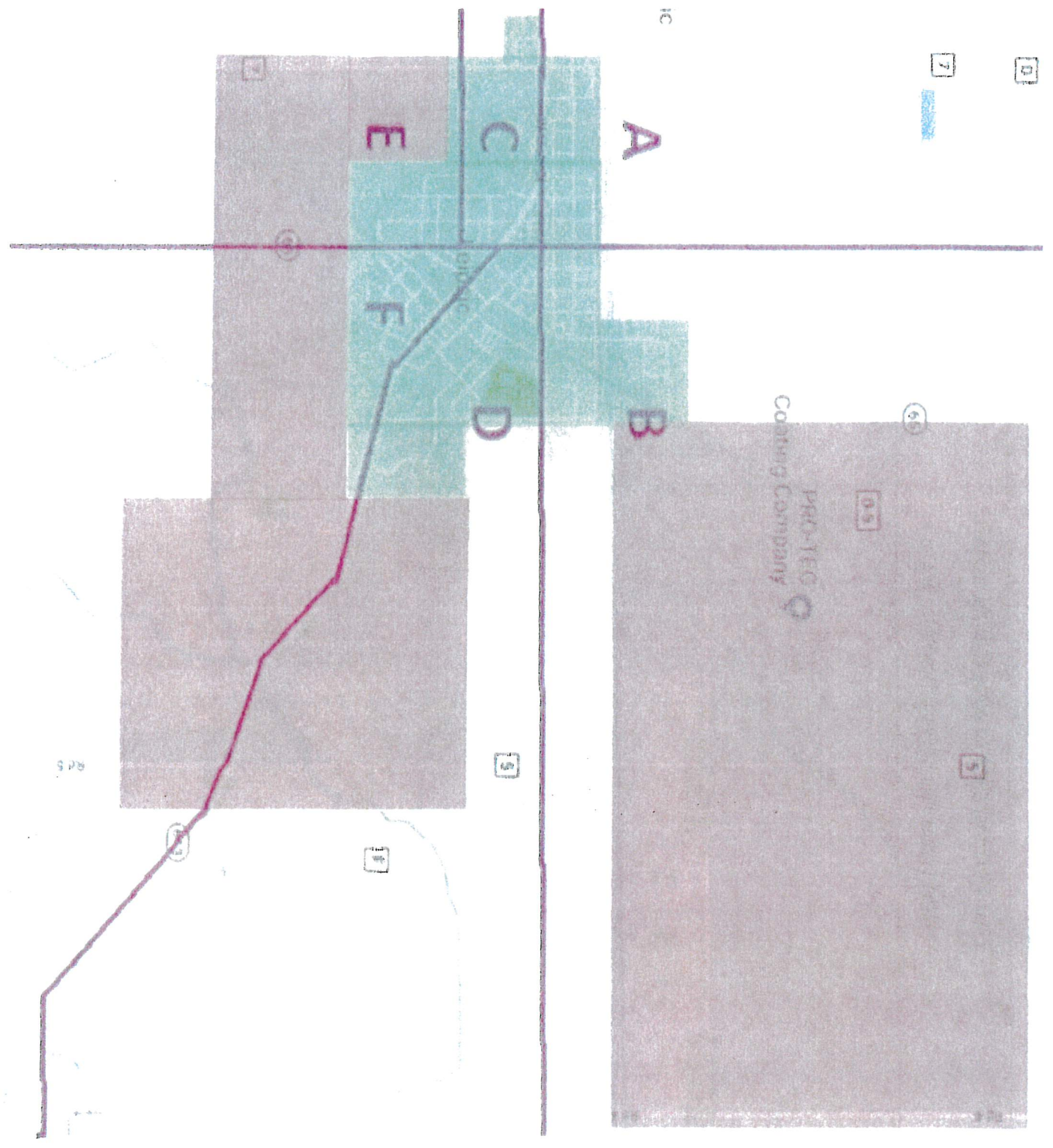
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The Properties Owned by Leipsic

Red Have No Sidewalks Yellow Do

